



# Board of Adjustment Staff Report Page 1

Meeting Date: October 2, 2025

Agenda Item: 9A

SPECIAL USE PERMIT CASE NUMBER: WSUP25-0013 (Sanctuary of God Church)

BRIEF SUMMARY OF REQUEST: To obtain a special use permit for a religious assembly use to allow the construction of a 15,000-square-foot single-story church at 3485 Rolling Ridge Road.

STAFF PLANNER: Eric Young, Senior Planner  
775.328.3613  
[eyoung@washoecounty.gov](mailto:eyoung@washoecounty.gov)

## CASE DESCRIPTION

For hearing, discussion, and possible action to approve a special use permit for a religious assembly use to allow the construction of a 15,000-square-foot single-story church which would include approximately 9,000 square feet of worship/assembly space (sanctuary, stage, and circulation) and 6,000 square feet of support areas, such as classrooms, offices, restrooms, and foyer. The project would also include a parking area for 154 parking spaces, utility infrastructure, and site grading in the amount of 18,000 cubic yards.

Applicant: Sierra Builders of Nevada  
Property Owner: Abner and Manuel Lopez  
Location: 3485 Rolling Ridge Road  
APN: 085-512-32  
Parcel Size: 5.003 acres  
Master Plan: Suburban Residential  
Regulatory Zone: Low Density Suburban (LDS)  
Planning Area: North Valleys  
Development Code: Authorized in Article 810, Special Use Permits  
Commission District: 5 – Commissioner Herman



## STAFF RECOMMENDATION

APPROVE

**APPROVE WITH CONDITIONS**

DENY

1001 E. Ninth St., Reno, NV 89512-2845

Telephone: 775.328.6100 – Fax: 775.328.6133

[www.washoecounty.gov/csd/planning\\_and\\_development](http://www.washoecounty.gov/csd/planning_and_development)

WSUP25-0013

SANCTUARY OF GOD CHURCH

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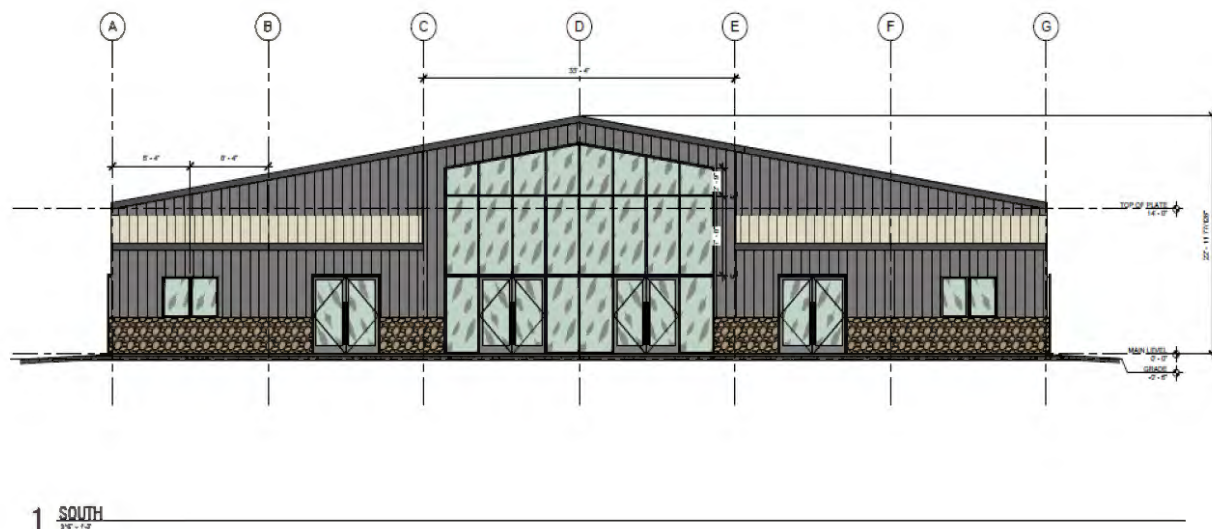
**Special Use Permit**

The purpose of a special use permit is to allow a method of review to identify any potential harmful impacts on adjacent properties or surrounding areas for uses that may be appropriate within a regulatory zone; and to provide for a procedure whereby such uses might be permitted by further restricting or conditioning them so as to mitigate or eliminate possible adverse impacts. If the Board of Adjustment grants an approval of the special use permit, that approval is subject to conditions of approval. Conditions of approval are requirements that need to be completed during different stages of the proposed project. Those stages are typically:

- Prior to permit issuance (i.e. a grading permit, a building permit, etc.)
- Prior to obtaining a final inspection and/or a certificate of occupancy on a structure
- Prior to the issuance of a business license or other permits/licenses
- Some conditions of approval are referred to as “operational conditions.” These conditions must be continually complied with for the life of the business or project.

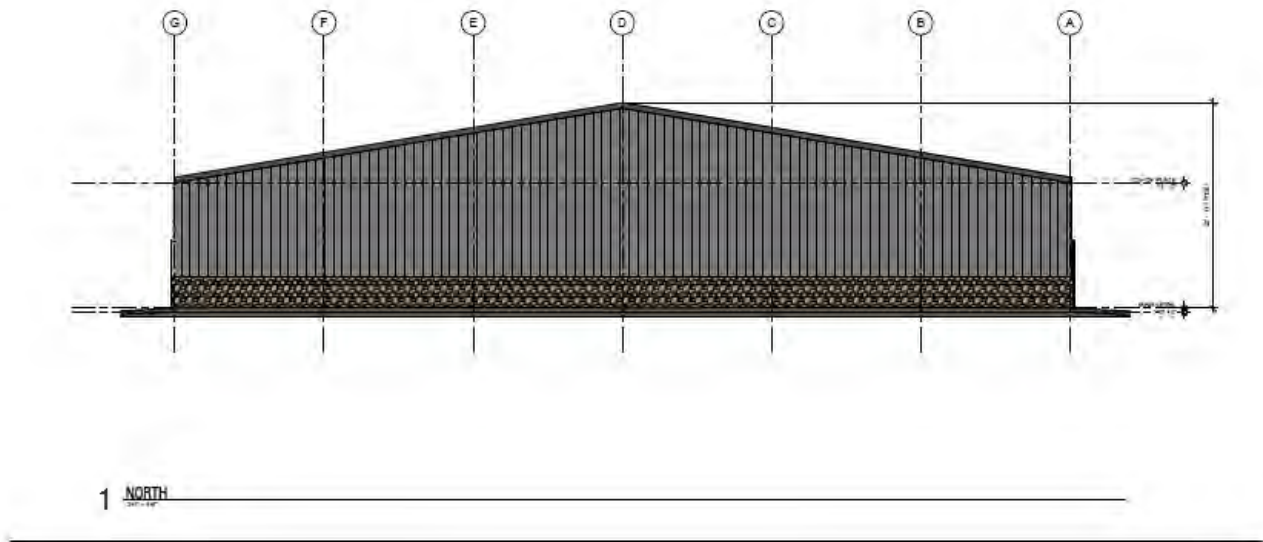
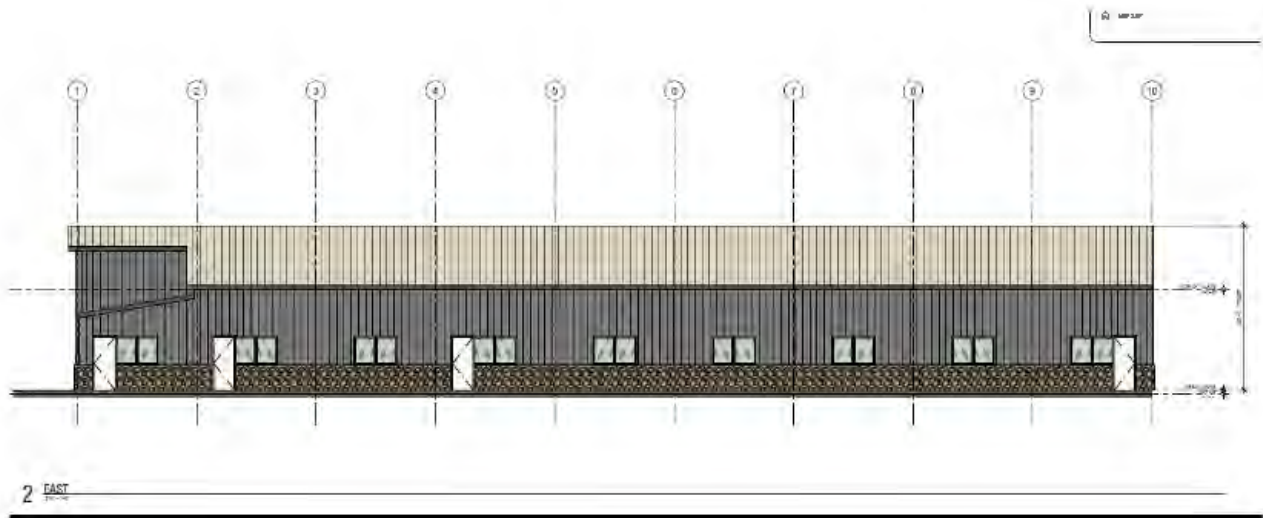
The conditions of approval for Special Use Permit Case Number WSUP25-0013 are attached to this staff report and will be included with the action order.

The subject property has a regulatory zone of Low Density Suburban (LDS). The proposed use of a church, which is classified as a religious assembly use-type, is permitted in LDS zoning with a special use permit per WCC Table 110.302.05.2 Civic Use Types. Therefore, the applicant is seeking approval of this SUP from the Board of Adjustment.



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**Site Plan and Elevations**

**Project Evaluation**

This special use permit is being requested to permit the construction of a church (religious assembly use-type) in the Golden Valley area of the North Valleys Planning Area. The proposed use would be located on a five-acre parcel on the cul-de-sac at the terminus of Rolling Ridge road. This parcel is one of six +/- 5-acre parcels generally surrounded by the 1-acre parcels of the Golden Valley Estates subdivision. The City of Reno boundary passes along the southern and eastern edges of the subject parcel. Edge matching, the technique of matching parcel sizes along jurisdiction residential boundaries, has resulted in these Reno parcels matching the 1-acre size of the unincorporated parcels found to the north and west of the subject parcel. These 1-acre and 5-acre residential parcels are consistent with the overall Golden Valley land use pattern.

The Washoe County Master Plan, including the North Valleys Area Plan, does not articulate any policies or action programs that would have an impact on the construction or operation of the proposed religious assembly use. The site will obtain water and wastewater services through a connection to the southeast utilizing existing infrastructure in the City of Reno.

The access road, Rolling Ridge Road, ends in a cul-de-sac at the subject property. The road has traditionally served the small residential neighborhood and has not experienced the periodic traffic increases that would result as a consequence of operating a religious assembly use on the subject parcel. It is likely this additional traffic will have an impact on the existing character of the neighborhood. However, the trip generation study submitted with the application (reviewed and approved by Washoe county engineering staff) resulted in approximately 50 peak hour trips. This is below the threshold of 80 trips required to generate a full traffic analysis.

The proposed site improvements that result in exceeding the major grading thresholds will result in an appropriate drainage system. While the amount of proposed grading triggers the threshold for an SUP, the proposed grading is typical of a development proposal of this size. The grading is intended to balance the site and result in a larger but more manageable buildable area. Overall, the development proposal will have access to adequate public facilities and services.

The proposed site is physically suitable for the proposed development. The proposed grading plan will result in a site that is physically prepared for the proposed development. The conditions of approval associated with grading and the standards of the development code will ensure this site is developed without harm to neighboring parcels. In general, the site is physically suitable for the proposed use.

The size and architectural style of the proposed building will likely be noticeable from the surrounding community. Public input generally expresses a concern about the elevation of the subject parcel, and how the proposed use will impact established vistas. The building itself is similar to a large, prefabricated metal barn utilized commonly in more rural areas. However, the proposed building contains numerous aesthetic improvements, including exterior siding and wainscoting, an earthtone based color palette, and expansive windows on the front. The small uniform windows on the sides and lack of windows on the rear may result in negative appearance of a monolithic block type structure more appropriate to large lot rural areas. While the development of this structure may impact the local vistas, it is not above and beyond the scale of structures a prospective homeowner could erect as by-right uses without any discretionary review.

Therefore, it is difficult to determine that the proposed religious assembly use would have a negative impact beyond the typical uses that could be developed by right on the subject parcel.

The additional traffic that the use is expected to generate is not significant enough to warrant a full traffic analysis. However, the additional traffic on Sundays and other days of full operation, even below the threshold for additional analysis, will certainly result in a change to the local community's character. The proposed use is fundamentally different from a residential use with accessory structures. The development and operation of the proposed church will impact the neighboring residential uses. However, religious assembly uses are typically established in or near residential neighborhoods and are permitted in all residential regulatory zones. The public input received for this project suggests the neighbors all believe the change will be negative. There is considerable concern regarding how the development and operation of the church will impact the existing community character. The public is particularly concerned about locating a large facility such as this church at the end of a dead-end cul-de-sac, with 100% of ingress and egress occurring on Rolling Ridge Road. Staff does not have any empirical data to indicate the nature of the change the project will bring to the surrounding area. Our code enforcement data shows that the religious assembly use type does not receive higher than average code enforcement complaints. In fact, code enforcement complaints regarding religious assembly are rare.

The parking, landscaping, lighting, and fencing depicted in the application are conceptual but provide a close approximation of what Washoe county code requires. The building plans submitted prior to construction will be in full conformance with Washoe County Code Articles 406, Building Placement Standards; 410, Parking and Loading; 412, Landscaping; and 414, Noise and Lighting. Staff has fashioned conditions of approval that seek to mitigate the anticipated impacts and result in a neutral or positive impact to the surrounding community character.

The nearest military installation is located at the Stead airport. The subject site and proposed use will not have an impact on this installation.

### **Neighborhood Meeting**

The applicant conducted a neighborhood meeting on August 20, 2025. A meeting summary and participant comments are attached in Exhibit C, Public Comment Letters. As discussed above, the neighborhood participants expressed concerns about traffic, scenic vistas, drainage and changes to the character of the surrounding area. The applicant responded with commitments to address the materials used on the outside of the structure to enhance its appearance, and to make sure the retention pond is designed appropriately.

### **Reviewing Agencies**

The following agencies/individuals received a copy of the project application for review and evaluation.

Agencies	Sent to Review	Responded	Provided Conditions	Contact
Environmental Protection	X			
NDOW (Wildlife)	X			
Washoe County Building & Safety	X			
Washoe County Parks & Open Space	X	X	X	Faye-Marie Pekar, fpekar@washoecounty.gov
Washoe County Water Rights Manager (All Apps)	X	X		
WCSO Law Enforcement	X	X		
Washoe County Engineering (Land Development) (All Apps)	X	X	X	Rob Wimer, rwimer@washoecounty.gov; Janelle Thomas, jkthomas@washoecounty.gov
Washoe County Engineering & Capital Projects Director (All Apps)	X	X	X	Dwayne Smith, desmith@washoecounty.gov; Stephe Hein, SHein@washoecounty.gov; Janelle Thomas, JKThomas@washoecounty.gov; Michon Reede, mreede@washoecounty.gov
NNPH Air Quality	X			
NNPH EMS	X	X		
NNPH Environmental Health	X			
TMFPD	X	X	X	Dale Way, dway@tmfpd.us;
Reno Community Development	X			
AT&T	X	X	X	Clifford Cooper, cc2132@att.com; Bryson Gordon, bg1853@att.com
NV Energy	X			
Utilities, Inc.	X			
Spectrum Cable	X			

All conditions required by the contacted agencies can be found in Exhibit A, Conditions of Approval.

### **Staff Comment on Required Findings**

WCC Section 110.810.30, Article 810, *Special Use Permits*, requires that all of the following findings be made to the satisfaction of the Washoe County Board of Adjustment before granting approval of the request. Staff has completed an analysis of the special use permit application and has determined that the proposal is in compliance with the required findings as follows.

- (a) **Consistency.** That the proposed use is consistent with the action programs, policies, standards and maps of the Master Plan and the North Valleys planning area.

**Staff Comment:** *The Washoe County Master Plan, including the North Valleys Area Plan, does not articulate any policies or action programs that would have an impact on the construction or operation of the proposed religious assembly use.*

- (b) **Improvements.** That adequate utilities, roadway improvements, sanitation, water supply, drainage, and other necessary facilities have been provided, the proposed improvements are properly related to existing and proposed roadways, and an adequate public facilities determination has been made in accordance with Division Seven.

*Staff Comment: The site has, or will have commensurate with development, the public facilities, services and infrastructure necessary to support the development.*

- (c) Site Suitability. That the site is physically suitable for religious assembly, and for the intensity of such a development.

*Staff Comment: The site is physically suitable for the proposed use. The standard development code regulations and the conditions of approval for the proposed grading will result in a site that is appropriate for the proposed use.*

- (d) Issuance Not Detrimental. That issuance of the permit will not be significantly detrimental to the public health, safety or welfare; injurious to the property or improvements of adjacent properties; or detrimental to the character of the surrounding area.

*Staff Comment: The standard development code regulations and the conditions of approval will ensure the permit is not detrimental to public health, safety, or welfare, nor injurious to the adjacent properties. The proposed development will impact the character of the surrounding area. The conditions of approval are intended to reduce the impacts of the proposed development.*

- (e) Effect on a Military Installation. Issuance of the permit will not have a detrimental effect on the location, purpose or mission of the military installation.

*Staff Comment: The proposed use at the subject location will not have any impact on the nearest military installation at the Stead airport.*

### **Recommendation**

After a thorough analysis and review, Special Use Permit Case Number WSUP25-0013 is being recommended for approval with conditions. Staff offers the following motion for the Board's consideration.

### **Motion**

I move that, after giving reasoned consideration to the information contained in the staff report and information received during the public hearing, the Washoe County Board of Adjustment approve with conditions Special Use Permit Case Number WSUP25-0013 Sanctuary of God, with the conditions included as Exhibit A to this matter, having made all five findings in accordance with Washoe County Code Section 110.810.30:

- (a) Consistency. That the proposed use is consistent with the action programs, policies, standards and maps of the Master Plan and the North Valleys Area Plan
- (b) Improvements. That adequate utilities, roadway improvements, sanitation, water supply, drainage, and other necessary facilities have been provided, the proposed improvements are properly related to existing and proposed roadways, and an adequate public facilities determination has been made in accordance with Division Seven;



- (c) Site Suitability. That the site is physically suitable for religious assembly, and for the intensity of such a development;
- (d) Issuance Not Detrimental. That issuance of the permit will not be significantly detrimental to the public health, safety or welfare; injurious to the property or improvements of adjacent properties; or detrimental to the character of the surrounding area;
- (e) Effect on a Military Installation. Issuance of the permit will not have a detrimental effect on the location, purpose or mission of the military installation.

**Appeal Process**

Board of Adjustment action will be effective 10 calendar days after the written decision is filed with the Secretary to the Board of Adjustment and mailed to the applicant, unless the action is appealed to the Washoe County Board of County Commissioners, in which case the outcome of the appeal shall be determined by the Washoe County Board of County Commissioners. Any appeal must be filed in writing with the Planning and Building Division within 10 calendar days from the date the written decision is filed with the Secretary to the Board of Adjustment and mailed to the applicant.

Applicant:                      Sierra Builders of Nevada; bob@sierrabuilders.biz



## Conditions of Approval

Special Use Permit Case Number WSUP25-0013

The project approved under Special Use Permit Case Number WSUP25-0013 shall be carried out in accordance with the conditions of approval granted by the Board of Adjustment on October 2, 2025. Conditions of approval are requirements placed on a permit or development by each reviewing agency. These conditions of approval may require submittal of documents, applications, fees, inspections, amendments to plans, and more. These conditions do not relieve the applicant of the obligation to obtain any other approvals and licenses from relevant authorities required under any other act.

**Unless otherwise specified**, all conditions related to the approval of this special use permit shall be met or financial assurance must be provided to satisfy the conditions of approval prior to issuance of a grading or building permit. The agency responsible for determining compliance with a specific condition shall determine whether the condition must be fully completed or whether the applicant shall be offered the option of providing financial assurance. All agreements, easements, or other documentation required by these conditions shall have a copy filed with the County Engineer and the Planning and Building Division.

Compliance with the conditions of approval related to this special use permit is the responsibility of the applicant, his/her successor in interest, and all owners, assignees, and occupants of the property and their successors in interest. Failure to comply with any of the conditions imposed in the approval of the special use permit may result in the institution of revocation procedures.

Washoe County reserves the right to review and revise the conditions of approval related to this Special Use Permit should it be determined that a subsequent license or permit issued by Washoe County violates the intent of this approval.

For the purpose of conditions imposed by Washoe County, "may" is permissive and "shall" or "must" is mandatory.

Conditions of approval are usually complied with at different stages of the proposed project. Those stages are typically:

- Prior to permit issuance (i.e., grading permits, building permits, etc.).
- Prior to obtaining a final inspection and/or a certificate of occupancy.
- Prior to the issuance of a business license or other permits/licenses.
- Some "conditions of approval" are referred to as "operational conditions." These conditions must be continually complied with for the life of the project or business.

**The Washoe County Commission oversees many of the reviewing agencies/departments with the exception of the following agencies.**

- **The DISTRICT BOARD OF HEALTH, through Northern Nevada Public Health (NNPH), has jurisdiction over public health matters. Any conditions set by NNPH must be appealed to the District Board of Health.**

FOLLOWING ARE CONDITIONS OF APPROVAL REQUIRED BY THE REVIEWING AGENCIES. EACH CONDITION MUST BE MET TO THE SATISFACTION OF THE ISSUING AGENCY.

**Washoe County Planning and Building Division**

1. The following conditions are requirements of Planning and Building, which shall be responsible for determining compliance with these conditions.

**Contact Name – Eric Young, Senior Planner, 775.328.3613, EYoung@washoecounty.gov**

- a. The applicant shall attach a copy of the action order approving this project to all permits and applications (including building permits) applied for as part of this special use permit.
- b. The applicant shall include a condition response memorandum with each subsequent permit application. That memorandum shall list each condition of approval, shall provide a narrative describing how each condition has been complied with, and the location of the information showing compliance with each condition within the improvement plan set that has been submitted.
- c. The applicant shall demonstrate substantial conformance to the plans approved as part of this special use permit.
- d. The applicant shall submit construction plans, with all information necessary for comprehensive review by Washoe County, and all applicable building permits shall be issued within two years from the date of approval by Washoe County. The applicant shall complete construction within the time specified by the building permits.
- e. A note shall be placed on all construction drawings and grading plans stating:

NOTE

Should any cairn or grave of a Native American be discovered during site development, work shall temporarily be halted at the specific site and the Sheriff's Office as well as the State Historic Preservation Office of the Department of Conservation and Natural Resources shall be immediately notified per NRS 383.170.

- f. Construction hours are 7am to 7pm Monday through Saturday. Any construction machinery activity or any noise associated with the construction activity are also limited to these hours.
- g. The applicant shall conduct a photometric analysis to confirm and ensure the lighting plan submitted with the building permit conforms with Article 414, Noise and Lighting Standards, and any other applicable county codes.
- h. The applicant shall erect one sign, consistent with Article 505 Signs, of the Washoe County development code, that directs visitors to the site not to park in the Rolling Ridge Road right of way.
- i. Outward facing temporary banner signs are prohibited. Temporary banner signs that are directed internally to the parcel are permitted.

- j. Final Landscape, parking, lighting, and fencing plans shall be in conformance with Washoe County development code articles 406, 410, 412, and 414.
- k. Development of the parcel shall not result in the relocation of existing utility infrastructure in the easement along the south parcel line.
- l. The following **Operational Conditions** shall be required for the life of the business:
  - i. This special use permit shall remain in effect until or unless it is revoked or is inactive for one year.
  - ii. Failure to comply with any of the conditions of approval shall render this approval out of conformance and subject to revocation.
  - iii. The applicant and any successors shall direct any potential purchaser/operator of the site and/or the administrative permit to meet with Planning and Building to review conditions of approval prior to the final sale of the site and/or the administrative permit. Any subsequent purchaser/operator of the site and/or the administrative permit shall notify Planning and Building of the name, address, telephone number, and contact person of the new purchaser/operator within 30 days of the final sale.
  - iv. This special use permit shall remain in effect as long as the religious assembly use is in operation.
- m. The following are conditions provided by the **City of Reno**, and shall be required to obtain site improvement or building permits:
  - i. The project proposes to connect to City of Reno sanitary sewer and shall meet Reno sewer standards and not Washoe County sewer standards
  - ii. A maintenance & access agreement/easement will be required for the private sewer to cross private property and connect to the public sewer main
  - iii. A sanitary sewer report will be required if the project proposes installing at least 200 drainage fixture units

### **Washoe County Engineering and Capital Projects**

2. The following conditions are requirements of the Engineering Division, which shall be responsible for determining compliance with these conditions.

**Contact Name** – Robert Wimer, P.E. (775) 328-2059

**General Conditions:**

- a. The applicant shall include a condition response memorandum with each subsequent permit application. That memorandum shall list each condition of approval, shall provide a narrative describing how each condition has been complied with, and the location of the information showing compliance with each condition within the improvement plan set that has been submitted.
- b. A complete set of construction improvement drawings, including an on-site grading plan, shall be submitted when applying for a building/grading permit. Grading shall comply with best management practices (BMP's) and shall include detailed plans for grading, site drainage, erosion control (including BMP locations and installation details), slope stabilization, and mosquito abatement. Placement or removal of any excavated materials

shall be indicated on the grading plan. Silts shall be controlled on-site and not allowed to be conveyed onto adjacent property.

**DRAINAGE (COUNTY CODE 110.416, 110.420, and 110.421)**

Contact Information: Robert Wimer, P.E. (775) 328-2059

**Conditions:**

- c. A detailed hydrology/hydraulic report, in conformance with the standards included in the Truckee Meadows Regional Drainage Manual, prepared by a professional engineer licensed in the State of Nevada shall be submitted to the Engineering Division for review and approval. The report shall include the locations, points of entry and discharge, flow rates, and flood limits of all 5- and 100-year storm flows impacting onsite and offsite areas and the methods for handling those flows. The report shall include all storm drain pipe and ditch sizing calculations, including a discussion of and mitigation measure design for any impacts on existing offsite drainage facilities and properties. Additionally, any increase in storm water runoff resulting from the development and based upon the 5- and 100-year storms shall be detained on site and attenuated to existing flow rates for discharge to the satisfaction of the County Engineer.
- d. The following note shall be added to the construction drawings; "All properties, regardless of if they are located within or outside of a FEMA designated flood zone, may be subject to flooding. The property owner is required to maintain all drainage easements and natural drainages and not perform or allow unpermitted and unapproved modifications to the property that may have detrimental impacts to surrounding properties."

**TRAFFIC AND ROADWAY (COUNTY CODE 110.436)**

Contact Information: Mitchell Fink, P.E. (775) 328-2050

**Conditions:**

- e. All roadway improvements necessary (including but not limited to, curb, gutter, sidewalk, signing and striping, driveway access, and street lighting) to serve the project shall be designed and constructed to County standards and specifications to the satisfaction of the County Engineer.
- f. Provide documentation of emergency/secondary access to the site to the satisfaction of the County Engineer.
- g. The minimum pavement structural section shall be four inches (4") of asphalt over six inches (6") of granular base for local and collector roadways and five inches (5") of asphalt over six inches (6") of granular base for bus routes and arterial roadways to the satisfaction of the County Engineer. All subgrade materials shall meet a minimum R-Value of 30 to the satisfaction of the County Engineer.
- h. The applicant shall submit a detailed geotechnical analysis and report for pavement design recommendations to the County Engineer for review and approval. The report shall be based on the estimated traffic loading for a 20-year design life and shall include assumptions concerning the distribution of trucks and buses, to include project construction truck traffic. The resultant pavement thickness in the geotechnical analysis shall be used if the report indicates a structural section that exceeds the minimum County standards, if required.
- i. Prior to ground-disturbing activity, a proposed Construction Traffic Haul Route Plan shall be submitted to the Engineering Division for review and approval. Any existing or proposed roads that will be used as construction haul routes and are not designated truck routes shall be evaluated by a geotechnical study to determine the existing structural section and



its load carrying capacity. If the pavement section is inadequate to support the proposed construction loading, the roadway shall be redesigned or reconstructed as needed to provide a 20-year design life in accordance with the AASHTO Interim Guide for Flexible Pavement.

- j. The applicant shall provide temporary traffic control plans for review and approval by the County Engineer prior to commencement of construction.
- k. The applicant shall submit an encroachment and excavation permit application for review and approval for any construction within Washoe County Right of Way.

**UTILITIES (County Code 422 & Sewer Ordinance)**

Contact Information: Katrina Pascual, P.E. (775) 954-4648

**Conditions:**

- l. The applicant shall conform to all conditions imposed by intergovernmental agreements required to provide sewer and reclaim water service to the subject project, and, if required, be a party to any such agreements.
- m. Coordinate with the City of Reno to connect to their existing sewer line.

**Truckee Meadows Fire Protection District**

- 3. The following condition is a requirement of the Truckee Meadows Fire Protection District, which shall be responsible for determining compliance with this condition.

**Contact Name – Dale Way; Deputy Fire Chief – Fire Marshal | Truckee Meadows Fire & Rescue, dway@tmfpd.us | Office: 775.326.6000**

- a. This project shall meet and comply with all requirements of currently adopted TMFPD fire codes, ordinances, and standards at the time of construction to include infrastructure for fire apparatus access roads and water supply. <https://tmfpd.us/fire-code/>

**Parks and Open Space**

- 4. Washoe County Parks and Open Space. The following condition is a requirement of Washoe County parks and Open Space, which shall be responsible for determining compliance with this condition.

**GENERAL CONDITIONS**

Contact Information: Faye-Marie Pekar, Park Planner. (775) 328-3623

- a. The application states that no export or import of material is anticipated with the proposed project. Should importation of earthen materials be necessary, those materials shall be “certified weed free” to prevent the spread of noxious weeds in Washoe County.
- b. All undeveloped disturbed areas of the site, including staging areas, shall be revegetated utilizing a native seed mix approved by Washoe-Storey Conservation District.

\*\*\* End of Conditions \*\*\*



Date: July 27, 2025

To: Eric Young, Senior Planner

From: Janelle K. Thomas, P.E., C.F.M., Senior Licensed Engineer  
Robert Wimer, P.E., Licensed Engineer

Re: Special Use Permit for **Sanctuary of God Churh WSUP25-0013**  
APN 082-512-32

### **GENERAL PROJECT DISCUSSION**

Washoe County Engineering staff have reviewed the above referenced application. The Special Use Permit is for the construction of a church and is located on approximately 5 acres on Rolling Ridge Road in Golden Valley. The Engineering and Capital Projects Division recommends approval with the following comments and conditions of approval which supplement the applicable County Code and are based upon our review of the site and the application prepared by Aryte Group LLC. The County Engineer shall determine compliance with the following conditions of approval.

For questions related to sections below, please contact the staff's name referenced.

### **GENERAL CONDITIONS**

Contact Information: Robert Wimer, P.E. (775) 328-2059

### **Conditions:**

1. The applicant shall include a condition response memorandum with each subsequent permit application. That memorandum shall list each condition of approval, shall provide a narrative describing how each condition has been complied with, and the location of the information showing compliance with each condition within the improvement plan set that has been submitted.
2. A complete set of construction improvement drawings, including an on-site grading plan, shall be submitted when applying for a building/grading permit. Grading shall comply with best management practices (BMP's) and shall include detailed plans for grading, site drainage, erosion control (including BMP locations and installation details), slope stabilization, and mosquito abatement. Placement or removal of any excavated materials shall be indicated on the grading plan. Silts shall be controlled on-site and not allowed to be conveyed onto adjacent property.

**DRAINAGE (COUNTY CODE 110.416, 110.420, and 110.421)**

Contact Information: Robert Wimer, P.E. (775) 328-2059

**Conditions:**

1. A detailed hydrology/hydraulic report, in conformance with the standards included in the Truckee Meadows Regional Drainage Manual, prepared by a professional engineer licensed in the State of Nevada shall be submitted to the Engineering Division for review and approval. The report shall include the locations, points of entry and discharge, flow rates, and flood limits of all 5- and 100-year storm flows impacting onsite and offsite areas and the methods for handling those flows. The report shall include all storm drain pipe and ditch sizing calculations, including a discussion of and mitigation measure design for any impacts on existing offsite drainage facilities and properties. Additionally, any increase in storm water runoff resulting from the development and based upon the 5- and 100-year storms shall be detained on site and attenuated to existing flow rates for discharge to the satisfaction of the County Engineer.
2. The following note shall be added to the construction drawings; "All properties, regardless of if they are located within or outside of a FEMA designated flood zone, may be subject to flooding. The property owner is required to maintain all drainage easements and natural drainages and not perform or allow unpermitted and unapproved modifications to the property that may have detrimental impacts to surrounding properties."

**TRAFFIC AND ROADWAY (COUNTY CODE 110.436)**

Contact Information: Mitchell Fink, P.E. (775) 328-2050

**Discussion:**

1. Prior to finalization of review of application, the applicant shall submit a revised trip generation analysis in compliance with Washoe County Standards. If the peak hour trip generation threshold of 80 peak hour trips is identified, the applicant shall be required to submit a detailed traffic report in conformance with Condition 2 below.

**Conditions:**

1. All roadway improvements necessary (including but not limited to, curb, gutter, sidewalk, signing and striping, driveway access, and street lighting) to serve the project shall be designed and constructed to County standards and specifications to the satisfaction of the County Engineer.
2. A detailed traffic report shall be prepared by a registered engineer and shall address driveway locations and turning movements, delivery truck patterns and movements, and provide recommendations on acceleration/deceleration lanes, storage lanes, and access control. The County Engineer shall be responsible for determining compliance with this condition and the traffic improvements that are required.
3. Provide documentation of access to the site to the satisfaction of the County Engineer.
4. The minimum pavement structural section shall be four inches (4") of asphalt over six inches (6") of granular base for local and collector roadways and five inches (5") of asphalt over six inches (6") of granular base for bus routes and arterial roadways to the satisfaction

of the County Engineer. All subgrade materials shall meet a minimum R-Value of 30 to the satisfaction of the County Engineer.

5. The applicant shall submit a detailed geotechnical analysis and report for pavement design recommendations to the County Engineer for review and approval. The report shall be based on the estimated traffic loading for a 20-year design life and shall include assumptions concerning the distribution of trucks and buses, to include project construction truck traffic. The resultant pavement thickness in the geotechnical analysis shall be used if the report indicates a structural section that exceeds the minimum County standards, if required.
6. Prior to ground-disturbing activity, a proposed Construction Traffic Haul Route Plan shall be submitted to the Engineering Division for review and approval. Any existing or proposed roads that will be used as construction haul routes and are not designated truck routes shall be evaluated by a geotechnical study to determine the existing structural section and its load carrying capacity. If the pavement section is inadequate to support the proposed construction loading, the roadway shall be redesigned or reconstructed as needed to provide a 20-year design life in accordance with the AASHTO Interim Guide for Flexible Pavement.
7. The applicant shall provide temporary traffic control plans for review and approval by the County Engineer prior to commencement of construction.
8. The applicant shall submit an encroachment and excavation permit application for review and approval for any construction within Washoe County Right of Way.

**UTILITIES (County Code 422 & Sewer Ordinance)**

Contact Information: Katrina Pascual, P.E. (775) 954-4648

**Discussion:**

1. The application states that the building will connect to the Washoe County public sewer system and comply with Washoe County standards. However, the existing sewer line that is being connected as shown on the plans is owned and operated by the City of Reno.

**Conditions:**

1. The applicant shall conform to all conditions imposed by intergovernmental agreements required to provide sewer and reclaim water service to the subject project, and, if required, be a party to any such agreements.
2. Coordinate with the City of Reno to connect to their existing sewer line.



Date: July 18, 2025

To: Eric Young, Senior Planner

From: Faye-Marie Pekar, Park Planner

Re: Special Use Permit WSUP25-0013 (Sanctuary of God Church)  
APN: 082-512-32

### GENERAL COMMENTS

Washoe County Park staff have reviewed the above referenced application. If approved, the special use permit would allow for religious assembly use to allow the construction of a 15,000-square-foot single-story church which would include approximately 9,000 square feet of worship/assembly space (sanctuary, stage, and circulation) and 6,000 square feet of support areas, such as classrooms, offices, restrooms, and foyer. The project would also include a parking area for 154 parking spaces, utility infrastructure, and site grading in the amount of 18,000 cubic yards. The County Park Planner shall determine compliance with the following conditions of approval.

For questions related to sections below, please contact the staff's name referenced.

### GENERAL CONDITIONS

Contact Information: Faye-Marie Pekar, Park Planner. (775) 328-3623

1. The application states that no export or import of material is anticipated with the proposed project. Should importation of earthen materials be necessary, those materials shall be "certified weed free" to prevent the spread of noxious weeds in Washoe County.
2. All undeveloped disturbed areas of the site, including staging areas, shall be revegetated utilizing a native seed mix approved by Washoe-Storey Conservation District.



**From:** [Way, Dale](#)  
**To:** [Young, Eric](#)  
**Subject:** WSUP25-0013 (Sanctuary of God Church) - Special Use Permit  
**Date:** Wednesday, July 16, 2025 1:03:08 PM

---

Eric,

“This event shall meet and comply with all requirements of currently adopted TMFPD fire codes, ordinances, and standards at the time of construction to include infrastructure for fire apparatus access roads and water supply.”

<https://tmfpd.us/fire-code/>

Thank you.

**Dale Way**

**Deputy Fire Chief – Fire Marshal | Truckee Meadows Fire & Rescue**

[dway@tmfpd.us](mailto:dway@tmfpd.us) | Office: 775.326.6000

3663 Barron Wy, Reno, NV 89511



*"Committed to excellence, service, and the protection of life and property in our community"*

Begin forwarded message:

**From:** "Michele D. Marquiz" <midamarq@gmail.com>  
**Subject:** Sanctuary of God Church Special Use Permit – Case Number  
**WSUP25-0013 Sanctuary of God Church – Tentative Hearing Date of 10/2/25**  
**Date:** September 7, 2025 at 9:55:30 PM MDT  
**To:** kjulian@washoecounty.gov, peter@cpnv.com, Leo Horishny  
<leohorishny@gmail.com>, rpierce@washoecounty.gov

Dear Washoe County Board of Adjustment Members:

I am writing this letter to you in regards to a proposal that has been put forth to build the Sanctuary of God Church (Special Use Permit Case Number [WSUP25-0013](#)), which will literally sit at the top of the 5-acre parcel directly behind my residence.

The Sanctuary is seeking to build a 15,000 square foot metal structure, standing 23 feet in height with grounds that include 167 parking spaces, light poles standing 20+ feet tall and vinyl fencing encompassing the entire perimeter. In our quaint community, and specifically in our Northstar Ranch and Rolling

Ridge neighborhoods we treasure our peace, our quiet and our solitude. Our residentially zoned community is a place where residents have worked hard to have sanctuaries of their own to enjoy life and our open rural surroundings. As the Google Maps Satellite image of the proposed site depicts (link below), it is by no means a suitable site as the immediate surrounding area includes residential-only structures seated on acre+ lots.

[Google Maps Link](#)



The very idea of having a Church with weekend services and weekday activities with up to 400 congregants attending is not an enhancement to our area, but rather a detriment to our way of life bringing heavy traffic, disrupting our quiet neighborhoods and raising multiple safety concerns.

The main entry/access point for the Sanctuary of God Church will be on Rolling Ridge Road, which is a very small street containing 11 homes with livestock on many of the lots. Rolling Ridge Road was not designed for two-way heavy traffic nor is it designed for an overflow parking lot for Churchgoers. It is a street where neighbors ride horses on the dirt shoulders of the road. It is also a street that is not plowed in the winter months so bringing heavy traffic through that artery with the street freezing after a snow storm is a recipe for disaster and a significant safety concern.



The proposed secondary emergency-only access/exit point is on Opal Station Drive, which aligns with a private road facing four homes. This is also a road that is not equipped for two way traffic or overflow parking nor is it plowed in the winter months, creating a disaster waiting to happen with increased traffic. The intersection of Golden Valley Road and Opal Station Drive is already a problem area for heavy traffic and increased traffic accidents.

The homes on Opal Station Drive are part of a residential planned community known as Northstar Ranch which contains 340 single family residences. There are only two entry/exit points within Northstar Ranch, which are off of Golden Valley Road as depicted below and at the end of Opal Station Drive through Chisholm Trail and Newport Lane.



In learning more details at the Neighborhood Meeting held on August 20, 2025, Youth Pastor (and the property owner of the proposed site) Abner Lopez noted that congregants would be traveling from Sparks, Sun Valley, Stead, Red Rock and Lemmon Valley in an effort to bring communities together. While the notion of bringing communities together is commendable, it would be at the expense of our residential neighborhood and rural community. There are many commercially zoned lots that would be ideal and far more appropriate logistically than to place the Sanctuary of God Church in the middle of homes in a community that does not want our peace and lifestyle disrupted with increased traffic, noise, pollution and crime, not to mention a decrease in property values by changing the dynamic and charm of our area.

In lieu of the Board of Adjustment members personally viewing the area, the pasted site images above clearly show that the proposed 5-acre Church site at 3485 Rolling Ridge Road is not a suitable site nor is it suitable architecturally in blending with the character of the surrounding area given the proposed metal building structure, large tiered parking lot and vinyl fencing. I respectfully ask the Board to please reject the Sanctuary of God Church proposal based on these merits.

Thanks and best regards,  
Michele Marquiz



Begin forwarded message:

**From:** Clyde Cordova <clycordova@gmail.com>  
**Subject: Opposition to Special Use Permit – Sanctuary of God Church**  
**Date:** September 19, 2025 at 4:08:35 PM PDT  
**To:** "kjulian@washoecounty.gov" <kjulian@washoecounty.gov>, "peter@cpnv.com" <peter@cpnv.com>, "leohorishny@gmail.com" <leohorishny@gmail.com>, "pcaldwell@washoecounty.gov" <pcaldwell@washoecounty.gov>, "rpierce@washoecounty.gov" <rpierce@washoecounty.gov>, "washoe311@washoecounty.gov" <washoe311@washoecounty.gov>  
**Cc:** Michael Marquiz <mmarquiz@gmail.com>

**To:**

[Kjulian@washoecounty.gov](mailto:kjulian@washoecounty.gov); [peter@cpnv.com](mailto:peter@cpnv.com); [leohorishny@gmail.com](mailto:leohorishny@gmail.com);  
[pcaldwell@washoecounty.gov](mailto:pcaldwell@washoecounty.gov); [rpierce@washoecounty.gov](mailto:rpierce@washoecounty.gov); [washoe311@washoecounty.gov](mailto:washoe311@washoecounty.gov)

**Subject:** Opposition to Special Use Permit – Sanctuary of God Church

Dear Board of Adjustment Members,

I'm writing as a longtime resident of Golden Valley, and I want to speak plainly about the proposed Special Use Permit for the Sanctuary of God Church.

This isn't about opposing faith or community. It's about protecting a neighborhood that was never designed to absorb institutional traffic. I live in a cul-de-sac with a single narrow road in and out. That road feeds directly into a busy artery—no traffic light, no turn lanes, and no infrastructure to handle the kind of volume this proposal would bring.

Under Section 110.810.30 of the Washoe County Development Code, you're required to find that the proposed use meets several criteria. Based on my experience living here, I believe the proposal fails to meet at least three of them:

- **(b) Improvements** – The area lacks the infrastructure to support institutional traffic: no traffic light, no turn lanes, and a narrow access road.
- **(c) Site Suitability** – A cul-de-sac with limited ingress and egress is not physically suitable for this intensity of use.
- **(d) Issuance Not Detrimental** – The project would be significantly detrimental to public safety, adjacent properties, and the character of our rural neighborhood.

Golden Valley isn't just where we live—it's how we live. We chose this area for its peace, its space, and its autonomy. A high-traffic institutional use like this church doesn't fit here. It's not compatible with the zoning, the infrastructure, or the spirit of the place.

There are other zones in Washoe County that can support this kind of development. Ours cannot. I urge you to listen to the people who live here, and to recognize that this proposal is incompatible, unsafe, and out of step with the character of our community.

Respectfully,  
Clyde Cordova  
Golden Valley Resident



Begin forwarded message:

**From:** Michael Marquiz <[mmarquiz@gmail.com](mailto:mmarquiz@gmail.com)>  
**Subject:** Sanctuary of God Church Special Use Permit – Case Number WSUP25-0013  
**Sanctuary of God Church – Tentative Hearing Date of 10/2/25**  
**Date:** September 15, 2025 at 9:37:45 AM PDT  
**To:** [kjulian@washoecounty.gov](mailto:kjulian@washoecounty.gov), [peter@cpnv.com](mailto:peter@cpnv.com), Leo Horishny <[leohorishny@gmail.com](mailto:leohorishny@gmail.com)>, [pcaldwell@washoecounty.gov](mailto:pcaldwell@washoecounty.gov), [rpierce@washoecounty.gov](mailto:rpierce@washoecounty.gov), [washoe311@washoecounty.gov](mailto:washoe311@washoecounty.gov)  
**Cc:** [jherman@washoecounty.gov](mailto:jherman@washoecounty.gov)

## Board of Adjustment Meeting October 2<sup>nd</sup>, 2025 1:30pm

### Washoe County Special Use Permit WSUP25-0013 (Sanctuary of God Church)

Dear Board of Adjustment Committee Members:

My name is Michael Marquiz and I am sending this in order to state my opposition to the Special Use Permit (SUP) for the Sanctuary of God Church Project based on the proposal's incompatibility with components (a), (b), (c), and (d) of section 110.810.30 in the Washoe County Development Code.

#### 110.810.30 (a) Consistency

The SUP is not consistent with the standards and maps of the applicable area plan which is zoned for low-density residential use and consists of 1+ acre custom/semi-custom homes on all adjacencies of the site; encompassing Golden Valley Road, Spearhead Way, Rolling Ridge Road, Opal Station Drive, and Opal Station Drive Private Road. Placing a large, 15,000 sq. foot, two-story sized commercial metal-structure with a giant parking lot right in the middle of several rural neighborhoods is neither consistent with the rural lifestyle of the adjacent neighborhoods nor does it fit in with the look and feel of the area. The residents within the adjacent neighborhoods selected this area for its peaceful lifestyle and breathtaking views, making this community our sanctuary. Granting the SUP would destroy the consistency of our cherished rural lifestyle.



Figure 1: 1+ Acre Custom/Semi-Custom Homes on All Adjacent Streets

#### 110.810.30 (b) Improvements

The SUP does not incorporate adequate roadway improvements and safety measures, or requisite drainage solutions; all of which would be required in order to accommodate the influx of traffic (pedestrian and vehicular) and prevent hazardous vehicular residue runoff from a large parking lot to infiltrate the water table and/or well systems of nearby or downstream neighborhoods.

Rolling Ridge Road (single entry/exit) and Opal Station Private Drive (secondary auxiliary access) as they currently exist are neither suitable for construction vehicular traffic nor the potential daily traffic to handle several hundred vehicles (based on 531 maximum occupancy, 400 congregants for services, and 154 parking spaces). Without significant improvements such as traffic lights, traffic signs, sidewalks, street lights, off-site community parking, and road-widening, none of the aforementioned streets are equipped to handle the increased traffic burden which would be incurred if the SUP is granted.



*Figure 2: The Intersection of Spearhead Way and Rolling Ridge Road*



*Figure 3: Single Entry/Exit via narrow Rolling Ridge Road*





*Figure 4: Rolling Ridge Road Part 2 of 4*



*Figure 5: Rolling Ridge Road Part 3 of 4*



Figure 6: Rolling Ridge Road Part 4 of 4





*Figure 7: Proposed Secondary Auxiliary Access via Opal Station Drive Private Road*



*Figure 8: Opal Station Drive Private Road Homeowners Own Land on Both Sides of the Road*

#### 110.810.30 (c) Site Suitability

The proposed site, with single-entry/exit of all construction and vehicular traffic occurring on a very small residential street will cause significant traffic congestion and safety concerns in many neighborhoods and on Golden Valley Road, Spearhead Way, Rolling Ridge Road, Opal Station Drive, Opal Station Drive Private Road, and Quartz Star Court. Overflow parking will impact existing neighborhoods and ease of access for emergency services to those neighborhoods. There are more suitable sites within the North Valleys which already have the roadway infrastructure and traffic controls in place to provide better, safer and easier access, with multiple entry and exit routes.

#### 110.810.30 (d) Issuance Not Detrimental

Granting this SUP would be detrimental to the quality of life for all of the residents in the adjacent and surrounding neighborhoods by allowing all of the following to occur:

- disrupting the peaceful rural lifestyles that the residents, livestock, domesticated animals, equestrian trail users, and wildlife inhabitants enjoy,
- placing a significant and unsustainable burden on our small residential neighborhood roads,
- bringing safety concerns and a potential uptick in crime as a result of the

increased vehicular and pedestrian traffic,

- creating a daily visual reminder – from all directions – of a large structure and tiered parking lot that do not fit in with the aesthetics of the surrounding residential neighborhoods,
- ushering in noise pollution and light pollution where it is not wanted,
- negatively impacting the property values, and
- introducing health risks through hazardous water runoff into the water table and wells, dust particulates from the extensive grading required during construction, exhaust fumes from increased traffic in the adjacent areas, and potential mosquito breeding grounds from the SUP's proposed water retention basin.

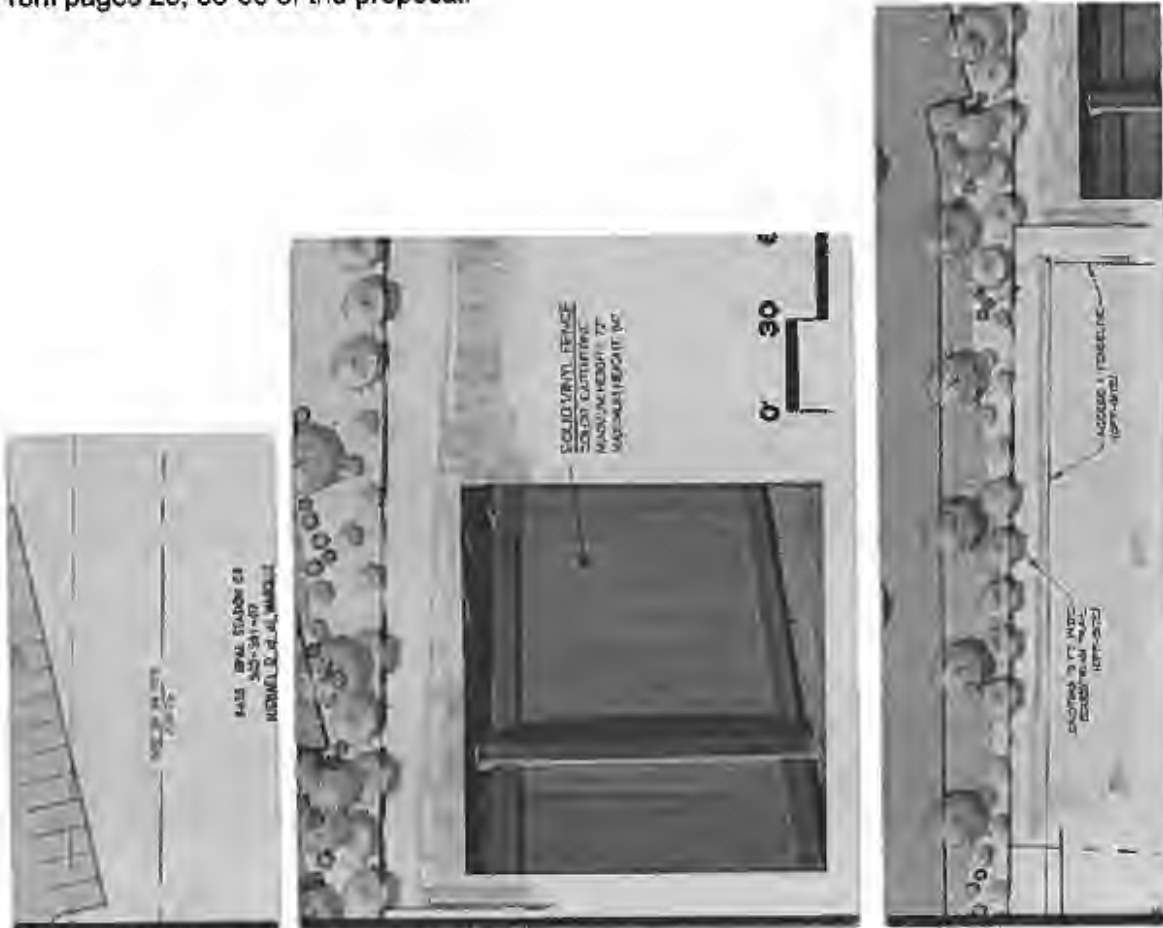
Based on all of these concerns and issues, I respectfully request that the Board of Adjustment deny this SUP for the Sanctuary of God Church.

Sincerely,

Michael Marquiz

Golden Valley Resident

From pages 23, 38-39 of the proposal:



**Questions:**

- Will the 15' equestrian trail be preserved in its entirety from Rolling Ridge Road to Opal Station Drive?
- Will the 6'-7' fence run the entire length of the equestrian trail from Rolling Ridge Road to Opal Station Drive?
- Will the 6'-7' fence surround the entire property?

From page 39 of the proposal:

The subject property is zoned Low Density Suburban (LDS). According to WCC 110.414.20(c): "For nonresidential uses permitted in residential zones, such as quasi-public uses, pole-mounted lighting shall not exceed 15 feet in height when located within 100 feet of a residentially zoned parcel." As such, all proposed pole lights:

- Are designed to be 12' within 100 ft and 20' outside of requirement, low poles are used near the perimeter where the site interfaces with residential uses.
- Are positioned within the interior of the site, away from residential property lines, primarily serving parking areas, drive aisles, and building entrances.

**Question:**

- How will the 100' placement of lighting take each property into account, and from where on each adjacent property will this be measured?



From page 9 of the proposal:

Vehicular access will be taken from Rolling Ridge Road, with internal drive aisles and parking designed to follow the natural slope of the land. The site includes 154 parking spaces, including ADA-accessible spaces and a fire apparatus turnaround. Parking is tiered to reduce visual and grading impacts. No shared access or cut-through traffic is proposed.

From page 16 of the proposal:

**Use of Existing Transportation Facilities and Improvements:** Explain how the project will make adequate use of existing infrastructure and not require the construction of new roads or facilities beyond what is approved in the Master Plan.

*Response: The project will take direct access from Rolling Ridge Road, an existing public street. No new public roads or off-site improvements are required. The anticipated traffic volume does not exceed thresholds for requiring a traffic impact analysis, and internal circulation has been designed in accordance with Washoe County standards. The site's layout and orientation allow for safe and efficient access without burdening surrounding transportation infrastructure.*

From Page 20 of the proposal:

**Access and Circulation**

The site provides controlled, single-entry access off Rolling Ridge Rd, with a secondary connection at Opal Station Drive to distribute vehicle circulation. Additionally, since no off-site grading is required

From page 28 of the proposal:

**Occupancy and Capacity**

The estimated occupancy of the building is 531 persons, calculated in accordance with Table 1004.5 of the International Building Code. This occupancy count does not include general circulation areas such as the arcade, which are not classified as occupiable space under IBC standards.

The breakdown includes:

- Worship area (6,000 SF at 1 occupant per 15 SF): 400 occupants
- Foyer area (1,666 SF at 1 per 15 SF): 111 occupants
- Office and breakroom areas (2,500 SF at 1 per 150 SF): 17 occupants
- Restrooms and accessory spaces (900 SF at 1 per 300 SF): 3 occupants

**Questions:**

- Has a traffic impact analysis been conducted by RTC to assess the impacts of overloading the traffic flows on Golden Valley Road and the arterial residential neighborhoods on Spearhead, Rolling Ridge Road, and Opal Station Drive?
- Where exactly is the proposed secondary connection at Opal Station Drive and will this secondary connection allow bidirectional traffic flow like on Rolling Ridge Road?
- How was it determined that Spearhead, Rolling Ridge Road and potentially Opal Station Drive are conducive to high-volume traffic?
- Are there plans to improve and convert Rolling Ridge Road from a residential road into a commercial road?
- What is the plan to mitigate overflow parking from occurring on neighborhood streets?
- Will sidewalks be added to the entry/exit points at Rolling Ridge Road and potentially Opal Station Drive?
- Are there plans for improving the roadway surfaces on Rolling Ridge Road and potentially Opal Station Drive?
- Will additional speed limit and caution signs, and street-lighting be placed on Spearhead and Rolling Ridge where the main traffic throughput will occur?



**FW: Sanctuary Church of God**

**From:** Steve Denny <Steve@sierrabuilders.biz>  
**Date:** Mon 8/25/2025 8:07 AM  
**To:** Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

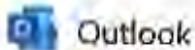
**From:** JUDY MACKAY <gandjmacKay@sbcglobal.net>  
**Sent:** Thursday, August 21, 2025 9:27 AM  
**To:** Steve Denny <Steve@sierrabuilders.biz>  
**Cc:** EYoung@washoecounty.gov <EYoung@washoecounty.gov>  
**Subject:** Sanctuary Church of God

Steve,

Spearhead and Rolling Ridge roads are not even striped which would not be safe for around 200 cars to be arriving or leaving at the same time. And since you only have parking for approximately 154 cars, where are the overflow cars going to park?

Also on page 19 of your application it states "metal siding and a darker standing seam metal roof" which do not comply with the Golden Valley or Opal Station neighborhoods.

Sincerely,  
Judy MacKay  
3415 Rolling Ridge Road  
Golden Valley



**Fw: Sanctuary of God - informal meeting**

**From** Steve Denny <Steve@sierrabuilders.biz>

**Date** Mon 8/25/2025 8:07 AM

**To** Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada

***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

**From:** Theresa Nannini <nannini@nvgbell.net>

**Sent:** Thursday, August 21, 2025 9:48 AM

**To:** Steve Denny <Steve@sierrabuilders.biz>

**Subject:** Sanctuary of God - Informal meeting

We are leaving the following comments about a SUP for this church asked for as a followup of initial meeting at Golden Valley High School.

1. Why would you say we had only to write on comment sheet our questions on same night (last night) and the say County needs in 3 days and if we don't reply by following morning comments won't be submitted to county? That gives you most of today and all day tomorrow to forward to County and gives the neighbors only a few hours.
2. We live on Rolling Ridge Road and this road is not suitable in our opinion for up to 500 members and over 156 vehicles (per parking lot) to go up and down our urban county road with 11 or 12 one acre custom homes and property using it on a daily basis. The amount of possible traffic on this culdesac road will directly impact the residents who live on Rolling Ridge Road as will the people.
3. The 5 acre parcel is the last at the end of Rolling Ridge Road culdesac. It is currently a Golden Valley county property with country living. The property access is the narrowest pie shaped piece of property that should not be as main access for such a large traffic area. If someone builds a home on it that is what it was intended for. Living in the county with horse/animal property.

**WSUP25-0013  
EXHIBIT C**

354. The structure will be higher than the existing 5 acre peak and will NOT be something that is conducive

to this valley.

5. The other concern is the traffic flow on Spearhead and Rolling ridge rd. and the intersections at Spearhead and Golden Valley dr. which currently is in need of a stoplight due to heavy traffic flow morning and evening when people are traveling to and from work and use Golden Valley as a main thorough fare. The intersection of Rolling ridge rd. and Spearhead is controlled by a stop sign on Rolling ridge rd. only, which doesn't prevent the traffic headed north on Spearhead from traveling at a high rate of speed as they proceed through that intersection. There should be a stop sign for traffic headed north on Spearhead at this intersection. Should approval be granted by the county for the proposed project it will greatly increase the traffic problems described and create a significant safety hazard for residents of this area.

Sincerely,

Tony & Theresa Nannini





**Fw: Questions HUB Metting- WSUP25-0013 Sanctuary of God**

From: Steve Denny <Steve@sierrabuilders.biz>

Date: Mon 8/25/2025 8:08 AM

To: Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

From: f.donshick@att.net <f.donshick@att.net>

Sent: Thursday, August 21, 2025 3:31 PM

To: Steve Denny <Steve@sierrabuilders.biz>

Cc: 'Young, Eric' <EYoung@washoecounty.gov>; 'David Madsen' <kriarchitecture@gmail.com>

Subject: Questions HUB Metting- WSUP25-0013 Sanctuary of God

Steve,

A lot of questions were put forth last night during the meeting. But per your request here are some of my other questions, concerns regarding WSUP25-0013 Sanctuary of God:

Overall feel that allowing the building of the church in the residential area chosen will ruin the quality of life and aesthetics that has existed for years in that area. Rolling Ridge Road is a small community street and is not designed for a large volume of traffic.

1. In your document on the County website in several places you state the direct/single access is off Rolling Ridge Road, and no shared access or cut-through is proposed, yet under the Civil Engineering Plan - Access and Circulation it states "The site provides controlled, single-entry access off Rolling Ridge Rd. with a secondary connection at Opal Station to distribute vehicle circulation." Is this an actual access point or will it be for emergency access only? Either way there needs to be some kind of secondary access to ensure egress, ingress if there is an emergency.
2. Question as to the justification of no traffic study, as the peak hour weekday trips are below 80, ... As the Sanctuary of God is primarily on Sunday (with up to 154 cars) shouldn't that be taken into account, especially as it is entering through a small neighborhood road?

- Page 38**
3. Under Findings and Justifications – Adequacy of Public Services it states “project will utilize existing public infrastructure, including sewer (Washoe County), electricity NV Energy... Water services by TMWA following annexation, and purchase of water rights.” Will this be off Opal Station area as everything on Rolling Ridge area is all wells and septic systems?
  4. Concern over the grading plans in that no information as to the soil on site being suitable for usage once it is moved around the property. What testing has been done if any to answer this?
  5. Concern that in the Development Plan Summary are you stating “The exterior features a natural-tone cultured wainscot with painted metal siding above and a darker standing seam metal roof.” That again does not fit in with the properties in the area.

Look forward to your responses to my and others questions and concerns.

Regards,  
Francine Donshick

## **Washoe County Project Proposal (WSUP25-0013)**

### **Sanctuary of God Church**

Questions Submitted by Oscar & Lorrie Aguilar Longtime Resident.

Questions:

#1: What Research/Studies was conducted to select this location for a Church?

> How many services on Sunday and how many study sessions do you have during the week?

> Where does the church anticipate the congregants coming from?

> Did the church take into consideration that there is no transit system available to this area?

#2: How was it determined that a small Cul-De-Sac Road in a rural area would be a good location for the church and its congregants? As the church will not be able to expand in this location.

> Why would the Church not consider a location more conducive to higher traffic and easier access for all congregants?

### **Traffic Concerns:**

**Rolling Ridge Rd is a small Cul-De-Sac,  
consisting of 11 homes.**

#1: What about the increased congestion, noise and safety the heavy traffic will have on Rolling Ridge Rd, Spearhead and East Golden Valley Rd?

#2: Has there been a study on what impact this will have on our small residential area that consists of 11 homes on a small Cul-De-Sac Street on Rolling Ridge Rd?

#3: Rolling Ridge Rd is a narrow Cul-De-Sac Road that cannot handle the traffic of a church with 154 parking spaces and what about the overflow traffic? (Capacity is 531 people, 400 people for service, if everyone travels in 2's that's approximately 200 cars)

> Where will the overflow of cars park?

## **Elevated Placement on Top of a Prominent Hill**

#1: The proposed facility would like to build on top of a prominent hill, if approved this will be an obstruction to all surrounding homes not only on Rolling Ridge Rd, but also Opal Station and Golden Valley Rd. This will intrude on the privacy of many homeowners who purchased their homes to have privacy and open lots to view the wildlife.

#2: We do not have streetlights on Rolling Ridge Rd, Spearhead or East Golden Valley Rd.

> What study/consideration was taken on how these light poles will impact are area?

#3: If the water runoff is set to go into retention ponds, and the ponds don't perk they will overflow and the oil, gas and other contaminants will end up in our wells.

> Has a study been done on the impact that the contaminated water runoff will have on the water wells?

#4: Has a traffic impact study been done to determine the impact of overloading the arterial residential neighborhoods on Rolling Ridge Rd, Spearhead and East Golden Valley Rd? As well as Opal Station if used as a secondary entrance?

#5: How will the constructions vehicles access the site without impacting the residential Cul-De-Sac roadway and obstructing the daily living of residents on Rolling Ridge Rd, Spearhead?





Outlook

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**Fw: Sanctuary of God Church - Comment**

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**From** Steve Denny <Steve@sierrabuilders.biz>

**Date** Mon 8/25/2025 8:08 AM

**To** Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

---

**From:** Emily Hagler <emilyrosehagler@gmail.com>

**Sent:** Friday, August 22, 2025 8:49 AM

**To:** Steve Denny <Steve@sierrabuilders.biz>

**Subject:** Re: Sanctuary of God Church - Comment

Thank you, can you clarify the exemption for a traffic study in the proposal. It says less than 80 trips are under the exemption. The road is so narrow it doesn't have a dividing line and if two full sized pickups drove in opposite directions you would have to slow down and move over to the edge of the road. I encourage you to drive up that street before submitting that recommendation. While I don't live on Rolling Ridge I do walk there with my toddler and it would be very dangerous without sidewalks and widening. I know that meeting was tense and I want to thank you for your professionalism. I will be contacting the county for more information on the proposed September 4th meeting.

Please include the following:

The nearest bus stop is over two miles away with no continuous sidewalk to access the church. If this proposed development is to be approved the developer needs to pay for sidewalks and road improvements for public safety.

Emily Hagler

On Fri, Aug 22, 2025 at 8:18AM Steve Denny <[Steve@sierrabuilders.biz](mailto:Steve@sierrabuilders.biz)> wrote:

Received, your comments (both emails) will be included.

Thank you.

(Sorry for the delayed response, I left very early yesterday morning to drive my daughter to college)

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

From: Emily Hagler <[emilyrosehagler@gmail.com](mailto:emilyrosehagler@gmail.com)>  
Sent: Thursday, August 21, 2025 9:44 AM  
To: Steve Denny <[Steve@sierrabuilders.biz](mailto:Steve@sierrabuilders.biz)>  
Subject: Re: Sanctuary of God Church - Comment

Steve,  
Please add the additional comment for the Sanctuary of God Church.

The Sanctuary of God Church is requesting a traffic analysis exemption. The narrow street of rolling rock does not have lines down the middle due to its size. A traffic analysis is absolutely needed for this project. Rolling ridge is not an adequate access road for a 400 person capacity church. This will create unsafe conditions for the residence of Rolling Ridge and Spearhead Way as well as the church's congregation. With no sidewalks this will be fatal.

Emily Hagler

On Wed, Aug 20, 2025 at 9:30 PM Emily Hagler <[emilyrosehagler@gmail.com](mailto:emilyrosehagler@gmail.com)> wrote:  
Please include the following comment for the sanctuary of god proposed development:

Emily Hagler

8840 Spearhead Way Reno, NV 89506

I have lived in my home for 10 years, and while I am a relatively new resident on this street, many of my neighbors have lived here for more than 30 years. This community is their forever home—and mine as well. The proposed church development would cause irreversible harm to this small rural neighborhood and the families who live here. The Golden Valley area is already overburdened by development, and the rural roads that serve it are unsafe. Intersections such as Spearhead and Golden Valley, and Opal Station and Golden Valley, are already dangerous, and it is only a matter of time before a serious accident occurs. While I understand that housing development is inevitable, the construction of a church in this location is not a necessity—it is **optional**.

Rolling Ridge is a quiet, close-knit community of retirees and families, located on a narrow, dead-end street. The increased traffic generated by a church would overwhelm this neighborhood, destroy its safety, and diminish the quality of life for everyone who lives here. Golden Valley Road, Spearhead, and Rolling Rock are already at maximum capacity, and traffic speeds on Spearhead have become so unsafe with the Beckworth development that I no longer feel comfortable letting my daughter play in our front yard, and have invested thousands of dollars into fencing to secure my home. The lack of sidewalks further endangers the children of North Valleys High School that walk in the area, making the risk of a tragic accident far too real. The church proposed to only operate on Sundays, however I have never known a church to not have additional activities daily. This omission of indented operations is a blatant fabrication.

In addition to traffic hazards, the proposed development raises serious environmental risks. A large parking lot will create significant runoff, contaminating the surrounding soil and groundwater. This area relies on well water, and pollutants from vehicle fluids, oil, and other debris could leach into the aquifer, threatening the health and safety of local residents. Such contamination cannot be reversed once it occurs.

There is also the issue of crime and public safety. Increased traffic and outside visitors—especially during evening and weekend events—will inevitably attract loitering, vandalism, and theft in what is now a quiet and low-crime neighborhood. Rolling Ridge residents, many of whom are elderly, should not have to live with the added stress of rising crime rates and the loss of security they have enjoyed for decades.

For all of these reasons—traffic hazards, environmental risks, public safety concerns, and the fundamental incompatibility with the character of this neighborhood—I strongly urge the County to deny the use permit for this proposed church. This community has worked hard to preserve its rural, residential nature, and it should not be sacrificed for an unnecessary development that will only harm the people who live here.

The meeting that was hosted at North Valley's high school was not adequate. Nor did anyone at the meeting let residents know about the county meeting proposed for September 4th.

Please confirm this will be submitted to the county.

Emily Hagler





**Fw: Sanctuary of God proposed location at 3845 Rolling Ridge Road in Golden Vally**

From Steve Denny <Steve@sierrabuilders.biz>

Date: Mon 8/25/2025 8:08 AM

To: Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

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**From:** Laurel aka Laurie Rodriguez <skaterlfr@hotmail.com>

**Sent:** Friday, August 22, 2025 11:11 AM

**To:** Steve Denny <Steve@sierrabuilders.biz>

**Subject:** Sanctuary of God proposed location at 3845 Rolling Ridge Road in Golden Vally

I am writing to you in regard to the above.

I cannot stress strongly enough that the location for this church is profoundly inappropriate.

It is in a strictly residential neighborhood, as supported by the historical and current zoning. The street that has been designated as the main access is barely wide enough to allow two cars to pass safely. Although the church has a parking lot planned, any overflow would be parked on Rolling Ridge. This would not allow the residents to park in front of their own houses. Emergency vehicles such as fire trucks would have to slow to a crawl to navigate the additional narrowing of the street.

This same situation also applies to Opal Station Road, where the secondary access is proposed.

The traffic before and after the services would choke the two streets for an extended time. Residents would be prevented from entering or leaving in a timely manner - especially if urgent medical services are required. Considering many of the residents on both Rolling Ridge Road and Opal Station Road are retirees, any delay could result in an adverse outcome. The church could possibly be held liable in such an instance.

The placement of the church makes no sense. The owner states his congregants come from Sun Valley, Lemmon Valley, and the Stead areas. So, common sense says, place your church where it is easily accessible from the freeway - not in a small, obscure location that is hard to find. When planting a new church, one goes to where the people are - not the reverse.

I can state with good conscience that no one from the neighborhood would be attending the church. Rather, it will be a continuous source of contention. This would be a drain on county resources, specifically the sheriff's department. Parking & noise are just a couple of problems that would have neighbors frequently calling for police services.

No one seems to consider that Rolling Ridge Road is rarely plowed during the winter. It is very steep in places and has led to cars sliding out of control in the past. The presence of a church will not lead to the street being plowed more often.

The egress points onto Golden Valley Road are dangerous now. With the average cost of a traffic light at \$1.2 million, is the church willing to pay \$2.4 million to install lights at Opal Station and Spearhead? In addition, the maintenance of the two lights would have to be paid by the church as it is not up to the taxpayer who did not request said lights.

The owner states that there will only be one service on Sunday. Yet, he states it is bilingual. That would indicate two services at least. In addition, I don't know of any church that does not have Bible Studies, youth groups, men's and women's groups, staff meetings, vacation bible schools, meetings with members of the congregation, and elder meetings, just to name a few. If "Sunday only" were the case, why does the building need offices? You could prepare sermons at home. Obviously, the statement that the church will be Sunday only is an outright lie.

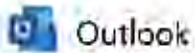
There was not one person from the owners' present congregation at the meeting in support of him or the project. That is not a good indication.

The bottom line is that the owner made a bad investment. He even admitted that he tried unsuccessfully to sell the land. Now, he is trying to make money by building a church.

The owner's actions show a blatant disregard for the residents in the neighborhood. It is all about what he wants with no thought to the people who live there - some as long as 50 years. Why would anyone want to build a church that is invasive and will never be welcome?

Again, I state that the proposed church location would be completely unsuitable.

Laurie Rodriguez



**Fw: Concerns regarding Special User Permit Application for Sanctuary of God Church**

From: Steve Denny <Steve@sierrabuilders.biz>

Date: Mon 8/25/2025 8:09 AM

To: Kara Bailey <kara@sierrabuilders.biz>

Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

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From: Elaine Hanford <ejhanford@att.net>

Sent: Saturday, August 23, 2025 1:26 PM

To: Steve Denny <Steve@sierrabuilders.biz>; David Madsen <kriarchitecture@gmail.com>; Eric Young <eyoung@washoecounty.gov>

Subject: Re: Concerns regarding Special User Permit Application for Sanctuary of God Church

I note that the Special Use Permit application is now scheduled for the 2 October 2025 agenda of the Board of Adjustment.

Case Number: WSUP25-0013 Sanctuary of God Church

Planner: Eric Young, eyoung@washoecounty.gov

Planning Area: North Valleys

Tentative Hearing Date: October 2, 2025

Reviewing Body: Board of Adjustment

Can you clarify when the written responses to public comments received during the Community Meeting of 20 August 2025 will be made available for review?

Thanks much,  
Elaine Hanford, PG-R, PhD

On Friday, August 22, 2025 at 08:20:04 AM PDT, Steve Denny <Steve@sierrabuilders.biz> wrote:

Received, your comments will be included.  
Thank you.



Steve Denney, Vice President of Construction  
Sierra Builders of Nevada  
***Celebrating 44 Years of Success / 1981-2025***



619-851-3386 (cell)

**From:** Elaine Hanford <ejhanford@att.net>  
**Sent:** Thursday, August 21, 2025 9:25 AM  
**To:** Steve Denny <Steve@sierrabuilders.biz>; Eric Young <eyoung@washoecounty.gov>; David Madsen <Kriarchitecture@gmail.com>  
**Cc:** Michael Marquez <mmarquiza@gmail.com>; Francine Donshick - GV POA President <f.donshick@att.net>  
**Subject:** Concerns regarding Special User Permit Application for Sanctuary of God Church

Thank you for the opportunity to present my concerns and questions regarding WSUP25-0013 Sanctuary of God Church application for Special Use Permit proposed for the roughly 5-acre property (parcel 082-515-32) at 3485 Rolling Ridge Road in our Golden Valley neighborhood.

Attached please find the written document that I provided at the Neighborhood Meeting held on Wednesday 20 August 2025 at the North Valleys High School. I look forward to the written response to each of my comments and concerns, as well as those raised by others attending the meeting.

If you have any questions or would like further clarification of my written concerns and comments, please do not hesitate to contact me.

Respectfully,  
Elaine J Hanford, PG-R, PhD



FW: Sanctuary of God Project / Neighborhood Meeting Comment Card

From bob@SierraBuilders.biz <bob@sierrabuilders.biz>

Date Thu 8/21/2025 2:24 PM

To Steve Denny <Steve@sierrabuilders.biz>; Kara Bailey <kara@sierrabuilders.biz>

FYI

Bob Cotter, President

Sierra Builders of Nevada

*Celebrating 43 Years of Success/1981-2024*



From: Clyde Cordova <clycordova@gmail.com>

Sent: Thursday, August 21, 2025 1:04 PM

To: bob@SierraBuilders.biz

Subject: Sanctuary of God Project / Neighborhood Meeting Comment Card

Neighborhood Meeting Comment Card

**Project Name:** Sanctuary of God / Golden Valley, Rolling Ridge Rd.

**Name:** Clyde & Martha Cordova

**Company Organization:** Resident

**Address:** 3465 Rolling Ridge Rd. Golden Valley

**Comment:**

**Church Development & Annexation Risk**

**Submitted by:** Clyde Cordova, Longtime Resident

**Subject:** Opposition to Proposed Church Development & Utility Connection to Reno



## 1. Public Safety & Crime Risk

Golden Valley is a quiet, low-crime neighborhood. The proposed development and emergency road would create direct access from Reno — a city with significantly higher crime rates.

- Reno's crime rate is 1.4× the national average, with frequent property crimes and organized youth theft.
- Increased traffic and visibility of rural properties may attract criminal activity.
- Elderly residents, livestock owners, and those with visible equipment are especially vulnerable.
- We request a **formal crime impact study** before any approval is granted.

## 2. Annexation Threat & Loss of Rural Zoning

The emergency road connects directly into Reno city limits. If the church is allowed to tap into Reno's water and sewer systems, it sets a precedent for annexation.

- Annexation would shift our zoning from Washoe County to Reno.
- Reno zoning may restrict livestock, fencing, and land use — threatening our rural lifestyle.
- Longtime residents built here for country living and autonomy. This development undermines that foundation.

## 3. Water System Shift & Financial Burden

We currently rely on private wells, supported by community investment and aquifer injection programs.

- Our neighborhood paid a **special assessment** to support TMWA's aquifer recharge — preserving older wells.
- Annexation would force us onto **metered municipal water**, with tiered rates and sewer charges.
- Lush landscaping and irrigation would become unaffordable, even if not prohibited.
- No compensation or rebate programs exist to offset this loss.

## 4. Livestock & Land Use Restrictions

If annexed, our properties may be rezoned under Reno's code, which could:

- Prohibit or restrict livestock
- Require conditional use permits or minimum lot sizes
- Limit fencing, outbuildings, and agricultural use

This would erase decades of rural tradition and investment.

## 5. Traffic, Emergency Access & Safety

The emergency road would funnel traffic directly into our quiet streets.

- Increased congestion, noise, and safety risks for pedestrians and animals
- No clear plan for traffic mitigation or emergency vehicle coordination
- Equestrian access and trail continuity could be disrupted

## 6. Tax Burden & Service Trade-Offs

49 Annexation would raise property taxes without guaranteeing proportional benefits.

- Higher taxes for water, sewer, and city services
- Loss of autonomy in managing wells, septic systems, and land use
- No clear plan for offsetting costs or preserving existing infrastructure

## 7. Community Investment Undermined

Golden Valley residents have invested in:

- Aquifer recharge through special assessments
- Livestock infrastructure
- Landscaping suited to well water
- A quiet, self-sufficient lifestyle

This development threatens to erase those efforts without meaningful compensation or protection.

## Elevated Placement & Neighborhood Impact

- The proposed facility will be built on **top of a prominent hill**, making it **highly visible from nearly every surrounding property**.
- Its elevated position means **no neighbor can avoid seeing it**—especially those with open acreage and unobstructed views.
- As a resident just **two lots down**, I will have a **direct line of sight** to the structure, which will dominate the landscape and **intrude on my privacy**.
- The building's **industrial metal exterior** is incompatible with the natural terrain and custom homes that define Golden Valley.
- This isn't just about aesthetics—it's about **property value, mental well-being, and the erosion of rural tranquility**.
- Unlike traditional churches that use **brick, stone, or architectural landscaping**, this proposal offers **no visual mitigation**—just raw metal on a hill.

## Personal Investment & Property Value Impact

Before I built my home in Golden Valley in 1995, I didn't just look at the land—I knocked on doors. I asked neighbors about their wells, their septic systems, and their experiences living here. But more than that, I wanted to know who the people were. After taking the time to listen and connect, my wife and I knew this was the right place to raise our family.

We didn't just buy a house—we made a long-term investment in a way of life. We put our savings and retirement funds into building a home that reflects our values: independence, sustainability, and community. That decision wasn't made lightly. It was rooted in trust—trust that the zoning, water systems, and rural protections would remain stable and respected.

At the time, the hill that is now Opal Drive was owned by the Bureau of Land Management (BLM). It was open, untouched, and part of the natural landscape that made this area so special. About ten years later, BLM traded that land to the City of Reno, which then sold it to developers. Now, we're facing a Conditional Use Permit (CUP) for a large metal building on that very hill—a structure that will be visible from every home in the neighborhood.

This building, intended as a church, lacks architectural integrity and community sensitivity. It resembles a warehouse or nightclub more than a place of worship. I pronounce Jesus and God as my God, and I believe any building erected in His name should reflect dignity, craftsmanship, and harmony with its surroundings—not industrial utility.

And once built, there's no guarantee it will remain a church. The CUP allows for flexibility in use, and the structure's commercial-grade design makes it easily convertible. If the church vacates or sells the property, the building could be flipped into a factory, event venue, or other high-impact use—none of which belong in a rural, residential neighborhood. This risk further undermines property values and community stability.

If this project is approved, I may be forced to consider selling my home before its value drops. The building's elevated placement, scale, and incompatibility with our custom homes will make Golden Valley less desirable to future buyers. Increased traffic, noise, and urban-style disruption will further erode the quiet lifestyle we've worked so hard to preserve.

This is not just a zoning issue—it's a matter of fairness, stewardship, and respect for the community that already exists here.

(original)

**Public Concerns & Comments – Proposed Sanctuary of God Church**

**20 August 2025**

EJ Hanford – phone: 775-677-8098 – resident of Golden Valley

I have reviewed the Sanctuary of God Church application for a Special Use Permit (Application) and have the concerns listed below that I believe must be fully responded to assist consideration of the Application for Special Use Permit by the County. Based on the currently available data and information and concerns, I believe the Special Use Application should be denied.

- Has a Geotechnical Investigation been conducted by a qualified Geotechnical Firm for the proposed project? If YES, then the Geotechnical Report should be made available in its entirety to determine whether it fully address the concerns listed below. If NO, then a Geotechnical Report needs to be conducted by a qualified Geotechnical Firm for the proposed project to fully address the concerns listed below. Note: I was told this morning (20 August 2025) in a telephone call with Robert Cotter that a geotechnical investigation has not yet been conducted.
- The geology of the site is mapped as epiclastic volcanic breccia that has been hydrothermally altered to quartz, sericite and clays (see attached figure). As shown on the attached figure, these geologic materials require engineered septic systems because they have failed percolation (infiltration) tests. Percolation (infiltration) tests should be conducted as part of the Geotechnical Investigation with regard to a proposed retention/detention pond.
- The Application indicate 18,000 cubic yards of material will be excavated on site, with the intent to use this material on site for fill (with no import or export). Has the suitability of this onsite material (hydrothermally altered volcanics resulting in significant clay content) been evaluated for onsite fill? For foundation support? For landscape vegetation?
- The Application indicates that 158,500 square feet (73%) of the roughly 5-acre property will be disturbed as indicated in Item 3 of the Supplemental Information Questionnaire. However, Item 2 under the Supplemental Information Overview indicates that roughly 80,000 square feet will be disturbed for building, parking and drainage improvements. These two numbers (158,500 versus 80,000) must be reconciled.



- How will the extent of disturbed area impact the amount of runoff on the site, especially since the Application indicates no retaining walls are being proposed and a significant proportion of the site will be covered by impermeable surfaces (e.g., building & parking area) that are known to increase runoff?
- A pond is proposed to "manage runoff and protect adjacent properties from erosion or increased flow." Throughout the text of the Application, this pond is referred to as a "detention pond." However, on Figure C2, it is labeled as a "retention pond." By definition, detention storage involves slowing runoff and then releasing it. Retention involves containing runoff and not releasing it downstream; retention waters are removed from storage only by infiltration through porous sediments at the bottom of the retention basin or by evaporation or evapotranspiration. The function of the onsite pond needs to be clarified. If it is a "detention pond" then how will waters be released? If it is a "retention pond" then questions of percolation become paramount along with concerns that pollutants could be transmitted to the groundwater or standing water residing in the pond could create environmental concerns associated with ponded stagnant water.
- Exact dimensions and capacity of this pond need to be provided, along with detailed explanation of how this pond was designed and the expected amount of runoff it will receive given that storms tend to be intense and localized rather than lighter precipitation. Has any percolation (infiltration) test been conducted in the proposed pond location? If so, what are the results? Given that the surrounding properties underlain by the same geologic material (i.e., hydrothermally altered volcanics with high clay content) were required by Washoe County to develop engineered septic systems (see attached figure), the viability of a pond in this location is questionable.
- There is additional concern that the pond will receive runoff contaminated with hydrocarbons and heavy metals from the parking lot areas. Infiltration of such contaminated water from the pond could contaminate groundwater that supplies domestic wells and drinking water for the adjoining and downgradient property residents.
- Further, there is concern that the pond may contain runoff for extended periods of time contrary to Washoe County Public Health environmental concerns, including the breeding of mosquitoes.

WASHOE COUNTY - 9505504 PROPERTY DATA	
<b>Owner Information</b>	
APN 002-012-32	Card 1 of 1
Site 1 3405 ROLLING RIDGE RD	Site #
WASHOE COUNTY NV 89500	
Owner 1 LOPEZ, ABRA	AC & (AND OTHERS)
Owner 2 or Trustee LOPEZ, MARIELA	
Real Address 862 TADPOLE CT	
SPARKS NV 89436	
<b>Parcel Information</b>	
Keyline Desc: FRAC NE4 SEC 14 TWP 20N RGE 19E (R05 1476)	
Subdivision: UNSPECIFIED	
Section 14 Township 20 Range 19	
Record of Survey Map: Parcel Map# Sub Map#	
Special Property Code	
2025 Tax 4000	Prior APN
District	
2024 Tax 4000	Tax Cap Use does not qualify for Low Cap, High Cap
District	Status Applied



Land Information		LAND DETAIL		
Land Use 120	DOR Code 120	Sewer None	Neighborhood GRAP	OB Neighborhood Plan
Size 217,930.68 Sqft	Size 5.005 Acres	Street Unpaved	Zoning Code LDS	
CAGC -		Water None		







Bonham HF and Binger EC. 1973. Geologic Map of the Reno Quadrangle. Nevada Bureau of Mines and Geology Map 4A, scale 1:24,000. Urban Map 4Ag. <https://pubs.blm.gov/arcsworld/arcsworld/geologic-map-of-the-reno-quadrangle-urban-4ag.html>

- Tertiary**
- Tb** Epishasta Volcanic Breccia. Greenish white volcanic breccia composed predominantly of lentic fragments derived from the erosion of rhyolitic flows and ash-flow tuff. In many areas the fragments are altered to quartz, sericite, and clay minerals.
  - Tbt** Hartford Hill Formation. Crystal-clear cream to buff rhyolitic ash-flow tuff with sparse crystals of quartz and feldspar in a moderately welded matrix of pumice and ash.



Engineered septic systems in volcanic terrain due to inadequate percolation.



From pages 23, 38-39 of the proposal:



**Questions:**

- Will the 15' equestrian trail be preserved in its entirety from Rolling Ridge Road to Opal Station Drive?
- Will the 6'-7' fence run the entire length of the equestrian trail from Rolling Ridge Road to Opal Station Drive?
- Will the 6'-7' fence surround the entire property?

From page 39 of the proposal:

The subject property is zoned Low Density Suburban (LDS). According to WCC 110.414.20(c):

"For nonresidential uses permitted in residential zones, such as quasi-public uses, pole-mounted lighting shall not exceed 15 feet in height when located within 100 feet of a residentially zoned parcel." As such, all proposed pole lights:

- Are designed to be 12' within 100 ft and 20' outside of requirement, low poles are used near the perimeter where the site interfaces with residential uses.
- Are positioned within the interior of the site, away from residential property lines, primarily serving parking areas, drive aisles, and building entrances.

**Question:**

- How will the 100' placement of lighting take each property into account, and from where on each adjacent property will this be measured?

From page 9 of the proposal:

Vehicular access will be taken from Rolling Ridge Road, with internal drive aisles and parking designed to follow the natural slope of the land. The site includes 154 parking spaces, including ADA-accessible spaces and a fire apparatus turnaround. Parking is tiered to reduce visual and grading impacts. No shared access or cut-through traffic is proposed.

From page 16 of the proposal:

**Use of Existing Transportation Facilities and Improvements:** Explain how the project will make adequate use of existing infrastructure and not require the construction of new roads or facilities beyond what is approved in the Master Plan.

*Response:* The project will take direct access from Rolling Ridge Road, an existing public street. No new public roads or off-site improvements are required. The anticipated traffic volume does not exceed thresholds for requiring a traffic impact analysis, and internal circulation has been designed in accordance with Washoe County standards. The site's layout and orientation allow for safe and efficient access without burdening surrounding transportation infrastructure.

From Page 20 of the proposal:

**Access and Circulation**

The site provides controlled, single-entry access off Rolling Ridge Rd, with a secondary connection at Opal Station Drive to distribute vehicle circulation. Additionally, since no off-site grading is required

From page 28 of the proposal:

**Occupancy and Capacity**

The estimated occupancy of the building is 531 persons, calculated in accordance with Table 1004.5 of the International Building Code. This occupancy count does not include general circulation areas such as the arcade, which are not classified as occupiable space under IBC standards.

The breakdown includes:

- Worship area (6,000 SF at 1 occupant per 15 SF): 400 occupants
- Foyer area (1,666 SF at 1 per 15 SF): 111 occupants
- Office and breakroom areas (2,500 SF at 1 per 150 SF): 17 occupants
- Restrooms and accessory spaces (900 SF at 1 per 300 SF): 3 occupants

**Questions:**

- Has a traffic impact analysis been conducted by RTC to assess the impacts of overloading the traffic flows on Golden Valley Road and the arterial residential neighborhoods on Spearhead, Rolling Ridge Road, and Opal Station Drive?
- Where exactly is the proposed secondary connection at Opal Station Drive and will this secondary connection allow bidirectional traffic flow like on Rolling Ridge Road?
- How was it determined that Spearhead, Rolling Ridge Road and potentially Opal Station Drive are conducive to high-volume traffic?
- Are there plans to improve and convert Rolling Ridge Road from a residential road into a commercial road?
- What is the plan to mitigate overflow parking from occurring on neighborhood streets?
- Will sidewalks be added to the entry/exit points at Rolling Ridge Road and potentially Opal Station Drive?
- Are there plans for improving the roadway surfaces on Rolling Ridge Road and potentially Opal Station Drive?
- Will additional speed limit and caution signs, and street-lighting be placed on Spearhead and Rolling Ridge where the main traffic throughput will occur?

Project Name: Sanctuary of God church - 111 Neighborhood Meeting

Meeting Location: north valleys HS

## SUMMARY

Meeting Date: 8/20/25

Virtual Meeting Option Provided: ☐ YES ☒ NO

Hosted By (Name): Steve Denney (Company): Sierra Builders

Contact (Email): Steve@sierrabuilders.biz (Phone): 619-851-3386

## Public Concerns:

1. Fit w/ neighborhood (residential) & Building materials
2. Traffic, road/infrastructure improvements
3. Annexation threat & loss of rural zoning
4. Constructability of soil, retention pms and water run off
5. Quality of life, crime, safety

## Changes Made to Proposal (if applicable):

1. Building exterior is Hardi(cement) siding w/ Rock wainscoting (no metal)
2. Updated rendering, will be created, showing site lines & added
3. architectural features
4. A traffic study will be done if required
5. our civil engineer will study retention pond to assure it is w/in code and will adequately serve the site.

## Any Additional Comments:

3.1- programs that will eventually be offered  
church- food pantry, AA, etc

5. traffic on rural streets. / winter  
hazards on Rolling Ridge, Golden Valley Rd.  
road closures.

6. impact on quality of life in neighborhood.  
Rural now adding traffic. Horses/wildlife  
Quietness.

7. Signage lighting / placement impact

Impact of headlights up + down  
homes that border the project  
Noise levels from the services, special  
events - no thank you!

Attachment C  
Page 60

This is a residential neighborhood!  
There have been fatalities on the G.V.R -  
what happens when you draw in more  
cars + traffic. Cars have uses Opel Statz  
Dr as a cut through - will this project  
bring more traffic into + through our  
neighborhood I've had a car run thru  
my front road - check with Police Report.  
We need less traffic through our neighbor  
hood speeding over the traffic calming humps

WSUP25-0013  
EXHIBIT C

Please confirm Receipt of my comment  
cared!

Neighborhood Meeting  
Comment CardProject Name: Sanctuary of God ChurchName: Michael Marquis

Company/Organization (if applicable): \_\_\_\_\_

Address: 8435 Opal Station Drive, Reno, NV 89506

Comment:

{ please see email  
sent to Babe@sierrabuilders.biz  
eyoung  
bosweld  
J. herman }



Neighborhood Meeting  
Comment CardProject Name: Sanctuary of God ChurchName: DAVE JIMERSON

Company/Organization (if applicable): \_\_\_\_\_

Address: 8375 Opal Station Dr.....  
Comment:

Why are you building a church in a residentially  
zoned neighborhood? THE HOMEOWNERS IN THIS  
COMMUNITY BOUGHT IN THIS AREA BECAUSE OF OPEN  
SPACE & LOW VOLUME TRAFFIC. WE WOULD MUCH  
RATHER SEE 5 LACRE HOMES THAN A CHURCH, IT  
WOULD BE A MUCH BETTER FIT FOR THE NEIGHBORHOOD.

Attachment C  
Page 63  
Neighborhood Meeting  
Comment Card

Project Name: \_\_\_\_\_

Name: JACK L. MASON

Company/Organization (if applicable): \_\_\_\_\_

Address: 8400 ORAL STATION DR

.....  
Comment:

① WILL THE CHURCH OFFER FREE PARKING FOR LOCAL RESIDENTS  
WHO MIGHT HAVE A FAMILY GATHERING (REUNION, WEDDING ETC)? ALSO,  
YOU SHOULD CONSIDER A PARKING LOT THAT CAN ACCOMMODATE LARGE  
TRUCKS NEEDING TO TURN

Project Name: Sacramento of Good Church

Name: Emily Hagler

Company/Organization (if applicable): \_\_\_\_\_

Address: 8840 Spearhead way Reno NV 89506

Comment:

Bollinger ridge + Spearhead can not handle +300  
cars, annexing into the city can greatly impact  
residence, traffic control studies, conditional  
use permit should be DENIED, stop lights,  
side walk, access all are not capable to handle  
the pressure. PLEASE reconsider allowing this  
development you are greatly impact EVERY  
person & from golden valley, Spearhead way, & Bollinger  
Rd.

Neighborhood Meeting  
Comment CardProject Name: SANCTUARY CHURCHName: Helen Lopez

Company/Organization (if applicable): \_\_\_\_\_

Address: 3400 Rolling Ridge Rd.....  
Comment:

1. Project not good for neighborhood  
No matter how many people ~~there~~ are supposed  
to be there you ~~cannot~~ cannot control how many  
people are going up and down Rolling Ridge
  2. Metal building will devalue our homes
  3. What does SANCTUARY mean? ILLEGALS?
- 65 NEVADA IS NOT A SANCTUARY STATE!



Neighborhood Meeting  
Comment Card

Project Name: Sanctuary of God Church

Name: Marilyn + Peter Fendlander

Company/Organization (if applicable): \_\_\_\_\_

Address: 8460 Opal Station Drive Reno NV

.....  
Comment:

- The application states there will be a  
secondary access onto Opal Station Dr.  
where will that connect into Opal Station Dr.?
- Will the church be annexed into the city? <sup>what is the</sup> plan?
- What if the church fails - does the new zoning stay  
and if so, what else could be built there?

Project Name: Sanctuary of God Church 3485RName: Katherine Clark

- 758487895

Company/Organization (if applicable): HomeownerAddress: 8310 Opal Station Dr Reno NV 89506Comment: See email from KMC - guitar & piano. comGolden Valley Rd / Opal Station Dr hazardous intersectionTRAFFIC COUNT. ILLUMINATIONProperty values lower with no tax breaksSound - Bells AmplificationAll hours - drugs alcohol etcRisk of home burglary

Neighborhood Meeting  
Comment CardProject Name: Sanctuary of God ChurchName: Carla Berryman jberryman2277 @ Att.net

Company/Organization (if applicable): \_\_\_\_\_

Address: 3515 E Golden Valley Rd Peru IN 49506

Comment:

1. How will the lighting affect the homes not on the OPAL Station side - Those on the Golden Valley side (we don't have street lights currently)
2. Will it <sup>property</sup> Annex into City w/ use of utilities @ OPAL Station \* Zoning impact
3. Church programming - Evenings etc. impact on neighborhood
4. Grading of property & impact on water runoff / wells on the Golden Valley property side that are on private wells, septic, and flood impact "down stream" contaminating ground water with runoff affecting wells  
percolation to city's main line

Project Name: Sanctuary of God Church Neighborhood Meeting  
Comment Card

Name: Pamela Egan email: eganpamela.j@gmail.com

Company/Organization (if applicable): \_\_\_\_\_

Address: 8395 Opal Station Dr

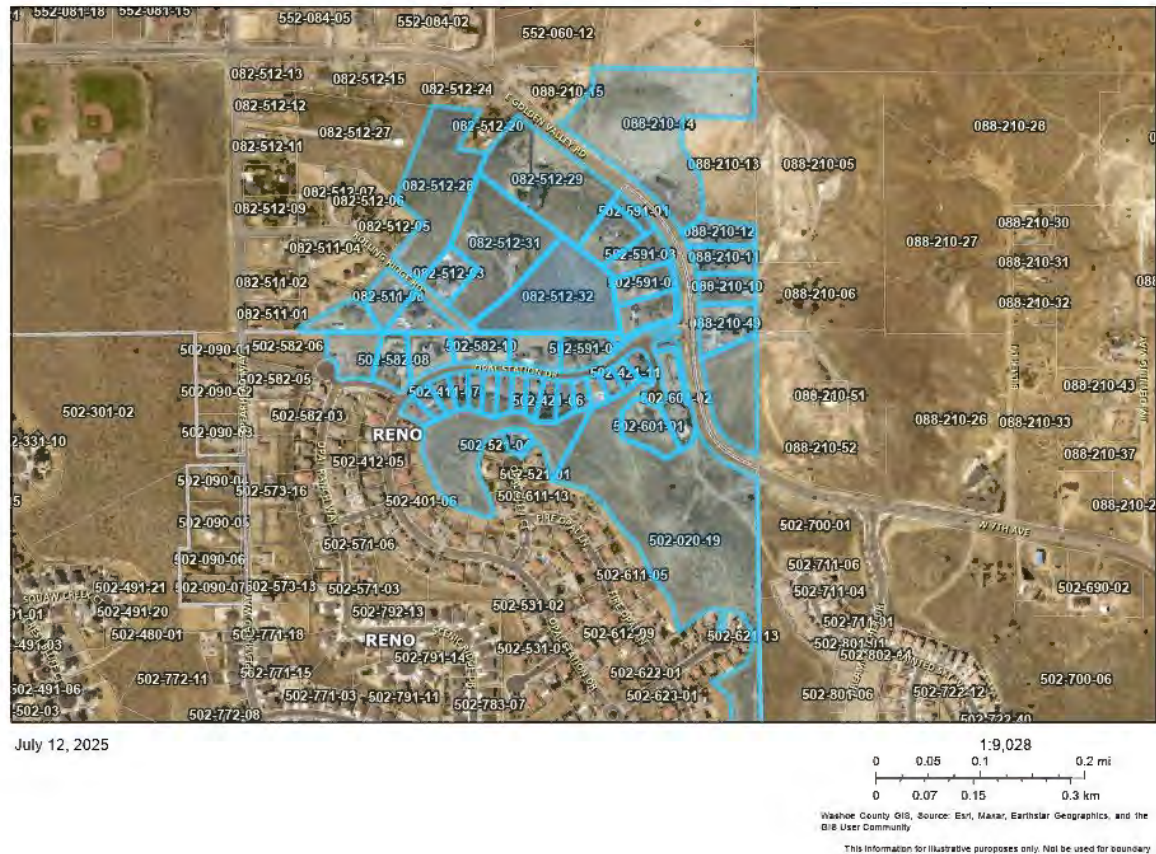
Comment:

This proposed project does not fit the  
low density neighborhood. Not parking  
spaces is Not what I want to look out  
at. One hundred cars on Rolling Ridge Rd  
will be a gross invasion into our neighbor  
hood, a safety issue at G.V. Road + R.R  
No - I do not want this in my backyard



## **Public Notice**

Washoe County Code requires that public notification for a variance must be mailed to a minimum of 30 separate property owners within a minimum 500-foot radius of the subject property a minimum of 10 days prior to the public hearing date. A notice setting forth the time, place, purpose of hearing, a description of the request and the land involved was sent within a 500-foot radius of the subject property. A total of 50 separate property owners were noticed a minimum of 10 days prior to the public hearing date.



## Public Notice Map

**Special Use Permit Case Number WSUP25-0013**



# Sanctuary of God Church

## Washoe County - Special Use Permit

Submittal Date: July 8, 2025





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# **Sanctuary of God Church SUP**

## **Section 1 - Application Material**

- Washoe County Development Application



## Washoe County Development Application

Your entire application is a public record. If you have a concern about releasing personal information, please contact Planning and Building staff at 775.328.6100.

<b>Project Information</b>		Staff Assigned Case No.: _____	
Project Name: <b>Sanctuary of God Church</b>			
<b>Project Description:</b> To initiate the Special Use Permit process for the establishment of a place of worship within the Low Density Suburban (LDS) zoning district, as required by Washoe County Development Code. The proposed development consists of a ±15,000 square-foot building on a ±5-acre site, including associated parking, access, and utility infrastructure.			
Project Address: 3485 Rolling Ridge Rd			
Project Area (acres or square feet): 5.003 acres total site-			
Project Location (with point of reference to major cross streets <b>AND</b> area locator):			
The project is located at the terminus of Rolling Ridge Rd. This is approximately 1,354 feet east of the intersection of Rolling Ridge Rd and Spearhead Way.			
Assessor's Parcel No.(s):	Parcel Acreage:	Assessor's Parcel No.(s):	Parcel Acreage:
082-512-32	5.003		
<b>Indicate any previous Washoe County approvals associated with this application:</b> Case No.(s). N/A			
<b>Applicant Information</b> (attach additional sheets if necessary)			
<b>Property Owner:</b>		<b>Professional Consultant:</b>	
Name: Lopez, Abner et al		Name: Aryte Group LLC	
Address: 882 Trebol Ct.		Address: 321 East 5th Street	
Sparks, NV	Zip: 89436	Reno, NV	Zip: 89501
Phone: 775-722-7776	Fax:	Phone: 801-413-8535	Fax:
Email:		Email: boswald@arytegroup.com	
Cell:	Other:	Cell: 801-413-8535	Other:
Contact Person:		Contact Person: Brooklyn Oswald	
<b>Applicant/Developer:</b>		<b>Other Persons to be Contacted:</b>	
Name: Sierra Builders of Nevada		Name:	
Address: 8755 Technology Way, Suite I		Address:	
Reno, NV	Zip: 89521		Zip:
Phone: 775-848-1734	Fax:	Phone:	Fax:
Email: bob@sierrabuilders.biz		Email:	
Cell:	Other:	Cell:	Other:
Contact Person: Bob Cotter		Contact Person:	
<b>For Office Use Only</b>			
Date Received:	Initial:	Planning Area:	
County Commission District:		Master Plan Designation(s):	
CAB(s):		Regulatory Zoning(s):	

February 2024

**Special Use Permit Application  
for Grading  
Supplemental Information**  
(All required information may be separately attached)

1. What is the purpose of the grading?

In association with the site improvements for a new Church

2. How many cubic yards of material are you proposing to excavate on site?

18,000

3. How many square feet of surface of the property are you disturbing?

158,500 of which all is paving, concrete, building, landscaping or rock stabilized.

4. How many cubic yards of material are you exporting or importing? If none, how are you managing to balance the work on-site?

None, the site has been designed to balance

5. Is it possible to develop your property without surpassing the grading thresholds requiring a Special Use Permit? (Explain fully your answer.)

No. Although the slopes on this site are less than 30% we would cross one threshold or another. ie: if the max fill height was under the threshold the export would exceed the threshold

6. Has any portion of the grading shown on the plan been done previously? (If yes, explain the circumstances, the year the work was done, and who completed the work.)

No

7. Have you shown all areas on your site plan that are proposed to be disturbed by grading? (If no, explain your answer.)

Yes



8. Can the disturbed area be seen from off-site? If yes, from which directions and which properties or roadways?

Yes, from all directions the site is visible and therefore any grading or site improvements could be seen. The disturbed areas will be developed and screened with landscaping.

9. Could neighboring properties also be served by the proposed access/grading requested (i.e. if you are creating a driveway, would it be used for access to additional neighboring properties)?

No, this is the last property in the area. The surrounding properties are already developed.

10. What is the slope (horizontal/vertical) of the cut and fill areas proposed to be? What methods will be used to prevent erosion until the revegetation is established?

The maximum slope onsite is a 2:1 and is limited to where necessary. These slopes are to be rip rapped or otherwise stabilized as recommended by geotech during final design.

11. Are you planning any berms?

Yes	NoX	If yes, how tall is the berm at its highest?
-----	-----	--

12. If your property slopes and you are leveling a pad for a building, are retaining walls going to be required? If so, how high will the walls be and what is their construction (i.e. rockery, concrete, timber, manufactured block)?

No walls are proposed.

13. What are you proposing for visual mitigation of the work?

Landscaping and/or rock stabilization is proposed for all disturbed areas along with landscape screening around the site.

14. Will the grading proposed require removal of any trees? If so, what species, how many and of what size?

No, there are no trees onsite.

15. What type of revegetation seed mix are you planning to use and how many pounds per acre do you intend to broadcast? Will you use mulch and, if so, what type?

Generic Upland Seed Mix, Hand broadcast 30.4 pls lbs/acre without mulch

16. How are you providing temporary irrigation to the disturbed area?

No temporary irrigation, permanent irrigation is proposed

17. Have you reviewed the revegetation plan with the Washoe Storey Conservation District? If yes, have you incorporated their suggestions?

No, but we have incorporated standard practices.

18. Are there any restrictive covenants, recorded conditions, or deed restrictions (CC&Rs) that may prohibit the requested grading?

Yes	NoX	If yes, please attach a copy.
-----	-----	-------------------------------

# **Sanctuary of God Church SUP**

## **Section 2 - Project Narrative and Justification**

- Project Narrative and Overview
- Site Location and Photos
- Project Request
- Site Plan and Development Limits
- Supplemental Information Overview
- Findings and Justification
- Traffic Analysis



## Project Narrative and Overview

The proposed project is the development of a place of worship on an approximately 5-acre vacant parcel located at 3485 Rolling Ridge Road in unincorporated Washoe County, Nevada. The property is zoned Low Density Suburban (LDS) and is located directly adjacent to the City of Reno boundary to the south.

The surrounding area is primarily composed of single-family residential homes situated on large lots, consistent with the LDS zoning district. The proposed use—a quasi-public facility (church)—is allowed in the LDS zone with the approval of a Special Use Permit (SUP). The development has been designed with sensitivity to neighborhood character, minimizing potential impacts through thoughtful site layout, landscaping, and buffering.

The parcel features ascending topography from west to east, creating a natural elevation change across the site. This characteristic has been used to inform the tiered site design, which reduces the visual and physical impact of grading. The parcel is currently undeveloped, offering a clean slate for integrated design that respects the site's physical constraints and surrounding context.

**Proposed Use:** The project consists of a ±15,000 square foot single-story church, including approximately 9,000 square feet of worship/assembly space (sanctuary, stage, and circulation) and 6,000 square feet of support areas, such as classrooms, offices, restrooms, and foyer. The building height will remain under 23 feet. Exterior materials—cultured stone and painted metal siding with a dark metal roof—have been selected to complement the residential neighborhood and provide a subdued, attractive presence.

Vehicular access will be taken from Rolling Ridge Road, with internal drive aisles and parking designed to follow the natural slope of the land. The site includes 154 parking spaces, including ADA-accessible spaces and a fire apparatus turnaround. Parking is tiered to reduce visual and grading impacts. No shared access or cut-through traffic is proposed.

The project provides a low-intensity, community-serving use in a location well-suited to accommodate it due to its size, topography, and separation from adjacent homes. All components of the design—including architecture, civil engineering, landscaping, and lighting—have been developed in compliance with Washoe County Code. The proposal ensures compatibility with surrounding residential uses and enhances community benefit while minimizing potential impacts.



## Site Location and Photos



General Location and Surrounding Context



View From Terminus of Rolling Ridge Road



View Looking North



## Project Request

This application requests approval of a Special Use Permit (SUP) for the development of a place of worship on an approximately 5-acre parcel located at 3485 Rolling Ridge Road in unincorporated Washoe County, within the Low Density Suburban (LDS) zoning district.

A place of worship is classified as a quasi-public use, which is permitted in the LDS zone subject to the approval of a Special Use Permit per Washoe County Development Code Section 110.302.05.1. The proposed project includes a  $\pm 15,000$  square foot single-story church with associated parking, landscaping, and site improvements. The design has been carefully planned to ensure compatibility with surrounding residential uses and to minimize potential impacts through appropriate buffering, lighting, and circulation design.

Additionally, the proposed grading activity associated with site development exceeds the thresholds established in Washoe County Code Section 110.438.35, thereby triggering the need for a grading-related Special Use Permit. Site grading will involve approximately 18,000 cubic yards of earth movement, with no import or export required. The site has been designed to balance grading on-site and preserve existing topography where feasible, with tiered parking, ADA access, and a detention basin integrated into the layout.

Therefore, this application seeks Special Use Permit approval to:

1. Establish a place of worship as a quasi-public use in the LDS zoning district.
2. Allow grading that exceeds the allowable volume and/or area thresholds requiring an SUP.

All proposed development will comply with applicable sections of the Washoe County Development Code, including but not limited to Chapters 110.302 (Allowed Uses), 110.438 (Grading), and 110.412 (Landscaping). Supporting narratives, civil and architectural plans, and technical documentation are included to demonstrate compliance and mitigation of potential impacts.



# Site Plan and Development Limits



## EXHIBIT LEGEND

-  DEVELOPED AREA
-  UNDEVELOPED AREA

## AREA DATA

PER Figure 110-412.25.1

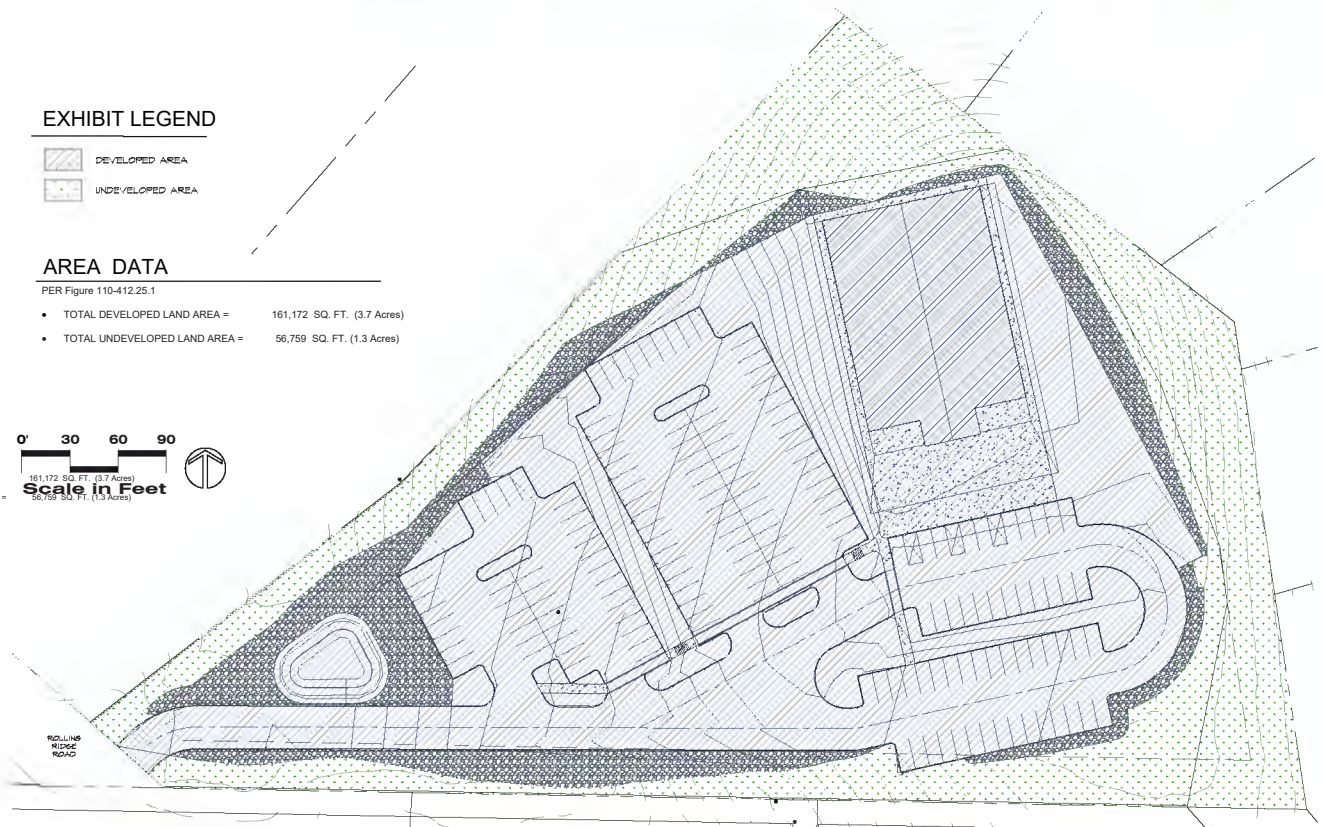
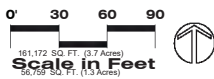
- TOTAL DEVELOPED LAND AREA = 161,172 SQ. FT. (3.7 Acres)
- TOTAL UNDEVELOPED LAND AREA = 56,759 SQ. FT. (1.3 Acres)

EXHIBIT LEGEND

## AREA DATA

PER 110-412.25.1

- AL DEVELOPED LAND AREA =
- AL UNDEVELOPED LAND AREA =





## Supplemental Information Overview

This narrative addresses the supplemental information and additional requirements found in the Washoe County Special Use Permit application guidelines (Community Services Department, Planning & Building), including the Supporting Information section (pages 7–9), for both the general SUP and grading SUP. The proposed church and grading plans meet all the supplemental information requirements and additional criteria for a Special Use Permit under Washoe County Code. Supporting documents—including architectural drawings, grading plans, landscaping plans, and technical memos—have been prepared to demonstrate compliance, minimize off-site impacts, and ensure compatibility with the surrounding neighborhood.

### SUPPLEMENTAL INFORMATION REQUIREMENTS – GENERAL SPECIAL USE PERMIT

1. **Description of the Proposed Use:** The project is for a quasi-public use (place of worship) including a church building with sanctuary, offices, classrooms, support spaces, parking, stormwater management, and landscape improvements.
2. **Location and Size of the Property:** The parcel is ±5 acres, located at 3485 Rolling Ridge Road, in the LDS zone of unincorporated Washoe County, adjacent to the City of Reno to the south.
3. **Current Zoning and Surrounding Uses:** Zoned LDS, the property is surrounded by single-family residential uses on large lots. The church use is a conditionally allowed quasi-public use under LDS zoning and is designed to be compatible with its residential surroundings.
4. **Physical Characteristics of the Site:** The site is vacant with gradual slope rising west to east. The design utilizes the natural topography to tier parking and minimize grading, with much of the site left undisturbed or revegetated with native seed mix.
5. **Hours and Days of Operation:** The facility will primarily operate on Sundays, with occasional weekday evening activities such as classes or meetings. There will be no overnight use or intensive daily activity.
6. **Expected Number of Employees and Attendees:** Services will accommodate up to 400 people, with approximately 3–5 weekday staff. The building design includes seating, support space, and circulation areas consistent with this level of use.
7. **Traffic, Access, and Parking:**  
Access is from Rolling Ridge Road. The proposed 154 parking spaces meet Washoe County requirements based on assembly occupancy and support uses. A traffic analysis exemption is justified (see separate trip generation memo), as peak-hour weekday trips are below 80. Circulation is designed for emergency vehicle turnaround, ADA accessibility, and safe internal movement.
8. **Lighting:** Lighting is designed for safety and security, with cut-off LED fixtures mounted at heights compliant with LDS standards (max 20 ft). Fixtures are shielded and downcast, and lighting will be on timers to reduce nighttime impact. There will be no light spillover onto adjacent residential properties.





## Supplemental Information Overview

9. Signage: A monument sign will be located at the site entrance, compliant with Washoe County standards for quasi-public uses in residential areas. Additional directional and informational signage will be located throughout the site for wayfinding and safety. A modest building-mounted identification sign will be provided. All signage is designed to be non-intrusive and in harmony with the neighborhood.

10. Landscaping and Screening: Landscaping meets WCC Chapter 110, Article 412, using drought-tolerant, regionally appropriate species with automatic irrigation. A landscape buffer including evergreen trees every 20 linear feet is provided adjacent to residential properties. Areas outside development footprints will retain undisturbed upland vegetation, and all disturbed slopes will be revegetated with the Upland Seed Mix as referenced in the Truckee Meadows BMP Handbook (2015).

### SUPPLEMENTAL INFORMATION REQUIREMENTS – SPECIAL USE PERMIT FOR GRADING

1. Cubic Yards of Material to Be Excavated: The site will require approximately 18,000 cubic yards of grading, exceeding the thresholds for requiring a grading SUP.
2. Surface Area to Be Disturbed: Roughly 80,000 square feet of the property will be disturbed for building, parking, and drainage improvements.
3. Import/Export of Material: No import/export is proposed. Grading will be balanced on-site.
4. Grading Thresholds Cannot Be Avoided: Due to the slope of the site and required cuts/fills for ADA access and fire access, the grading thresholds cannot be avoided.
5. Previous Grading: No prior grading has been performed on this parcel.
6. Visibility from Off-Site: Yes, the site is visible from surrounding parcels and Rolling Ridge Road. Impacts will be mitigated with screening landscaping, natural transitions, and non-intrusive lighting and architecture.
7. Slope and Erosion Control: Maximum slopes are 2:1, stabilized with riprap per the geotechnical engineer's recommendations. Erosion control will follow County BMPs.
8. Retaining Walls: No retaining walls are proposed.
9. Visual Mitigation: Landscaping and revegetation of disturbed areas will provide visual mitigation. Building design and lighting also reduce visual intrusion.
10. Tree Removal: No trees are present on site; no tree removal is needed.
11. Revegetation: Disturbed areas will be hand-broadcasted with 30.4 PLS lbs/acre of Upland Seed Mix, with permanent irrigation provided for plant establishment. No mulch is proposed.
12. Conservation District Review: Not yet reviewed by Washoe-Storey Conservation District; however, standard revegetation practices per the BMP Handbook are included.
13. Restrictive Covenants: There are no known CC&Rs or deed restrictions that would restrict the proposed grading.
14. Shared Access: Access and grading are self-contained. There is no shared or future access required for neighboring parcels. A utility access easement will be required.

## Findings and Justification

### Required Findings per Washoe County Development Code Section 110.810.30

**Plan Consistency:** Explain how the project is consistent with the policies and action programs of the Master Plan and the applicable Area Plan.

*Response: The project is consistent with the Washoe County Master Plan and the applicable Area Plan policies that support community-serving facilities, such as places of worship, within residential areas where appropriate infrastructure and buffering are provided. The LDS (Low Density Suburban) zoning allows quasi-public uses—including religious institutions—subject to Special Use Permit approval. The site is located within an area designated for low-intensity uses, and the proposed church aligns with the County's vision of providing neighborhood-scaled, service-oriented uses that enhance community well-being.*

**Land Use Compatibility:** Explain how the proposed use is compatible with existing and allowed uses in the surrounding area and will not adversely impact nearby properties.

*Response: The surrounding area consists primarily of single-family residential homes on large lots, consistent with LDS zoning. The proposed place of worship is a low-impact, quasi-public use that is compatible with the residential character of the area. The project has been designed with substantial setbacks, landscape buffers, and architectural treatments to minimize visual, traffic, and noise impacts. Lighting is shielded and downcast, and outdoor activity areas are internal to the site. No adverse impacts to nearby properties are anticipated.*

**Site Suitability:** Explain how the site is physically suitable for the type, intensity, and scale of the development.

*Response: The parcel is approximately 5 acres in size and features ascending topography from west to east. The size and shape of the parcel provide adequate space for a modest-sized building ( $\pm 15,000$  sq ft), circulation, tiered parking, landscape screening, and stormwater management. The site design respects existing grades, minimizes earthwork, and avoids overdevelopment. No critical slopes or natural habitat areas are impacted. The project fits comfortably within the site and surrounding context.*

**Non-Detriment:** Explain how the project will not be detrimental to public health, safety, or welfare.

*Response: The project has been designed to comply with all applicable Washoe County codes and standards for fire access, drainage, lighting, and traffic. Civil design includes an on-site detention basin, compliant ADA access, and a turnaround for emergency vehicles. Lighting will meet County code and will not result in light spillover. Grading is balanced on site and mitigated through best management practices. As such, the project will not pose any threat to public health, safety, or welfare.*



## Findings and Justification

**Adequacy of Public Services:** Explain how the project will not place an excessive burden on existing public services and infrastructure.

*Response: The project will utilize existing public infrastructure, including sewer (Washoe County), electricity (NV Energy), and telephone/data services. Water service will be provided by Truckee Meadows Water Authority (TMWA) following annexation, and the applicant will acquire necessary water rights. Fire protection will be provided by the nearest Washoe County Fire Station. The church's intermittent use (primarily Sundays and limited weekday activities) will not place a sustained burden on public services or infrastructure.*

**Natural Resource Conservation:** Explain how the project demonstrates appropriate conservation of energy and natural resources.

*Response: The development minimizes grading through a balanced earthwork design and follows existing topography. Drought-tolerant landscaping and native revegetation will be installed consistent with Washoe County standards. Irrigation will be efficient and limited to new planting zones. The building will use LED lighting and energy-efficient systems. No protected natural resources will be impacted, and stormwater will be managed on-site.*

**Use of Existing Transportation Facilities and Improvements:** Explain how the project will make adequate use of existing infrastructure and not require the construction of new roads or facilities beyond what is approved in the Master Plan.

*Response: The project will take direct access from Rolling Ridge Road, an existing public street. No new public roads or off-site improvements are required. The anticipated traffic volume does not exceed thresholds for requiring a traffic impact analysis, and internal circulation has been designed in accordance with Washoe County standards. The site's layout and orientation allow for safe and efficient access without burdening surrounding transportation infrastructure.*



## Traffic Analysis

This summary evaluates anticipated traffic impacts associated with a proposed place of worship located at 3485 Rolling Ridge Road. The project consists of a ±15,000 square foot building including approximately 9,000 square feet of A-3 assembly area (worship hall, stage, seating) and 6,000 square feet of B-occupancy support space (offices, classrooms, restrooms, foyer). The proposed development is subject to Washoe County's SUP requirements. Per Washoe County Code: Traffic impact reports are required whenever the proposed development project will generate 80 or more weekday peak hour trips as determined using the latest edition of the Institute of Transportation Engineers (ITE) Trip Generation Manual.

### Trip Generation Analysis:

Trip generation estimates are based on the ITE Trip Generation Manual, 11th Edition, Land Use Code 560 – Church. These trip volumes are significantly below the 80-trip threshold and therefore exempt from further Traffic Impact Report requirements under Washoe County Code.

Time Period	ITE Rate (per 1,000 SF)	Project Size (SF)	Estimated Trips
Weekday PM Peak	0.49 trips	15,000 SF	7.4 trips
Saturday Peak	0.68 trips	15,000 SF	10.2 trips
Sunday Peak	~0.75 trips (est.)	15,000 SF	11.25 trips

### Multi-Passenger Mitigation Measures:

Places of worship trips often involve more than one passenger. In addition, programming is structured to avoid traffic surges:

- Primary services occur on weekends, with limited weekday use.
- Office and classroom uses generate minimal traffic, typically outside of standard commuter peak periods.
- Parking is tiered and internal circulation has been designed to ensure safe, efficient vehicle flow.

The proposed place of worship will generate well under 80 peak hour trips, as confirmed by ITE trip rates and site-specific programming. Shared transportation, minimal weekday demand, and staggered activity periods ensure the project will not have significant off-site traffic impacts.

No formal Traffic Impact Report is required for this application.



# **Sanctuary of God Church SUP**

## **Section 3 - Development Plans**

- Development Plan Summary
- Civil Engineering Narrative and Plans
- Architectural Narrative and Plans
- Landscape Narrative and Plan
- Lighting Narrative and Plan



## Development Plan Summary

The design thoughtfully integrates architecture, civil engineering, landscape, and lighting to ensure compatibility with surrounding residential uses and minimize adverse impacts.

The proposed building is a single-story structure under 23 feet in height, approximately 15,000 square feet in size. It includes 9,000 square feet of A-3 assembly space for worship, seating, and stage use, and 6,000 square feet of B-occupancy support areas including offices, classrooms, foyer, and restrooms. The exterior features a natural-tone cultured stone wainscot with painted metal siding above and a darker standing seam metal roof. Storefront glazing on the south elevation defines the main entry, while other elevations include office window openings and contextual glazing to soften the structure within the neighborhood.

The site grading has been carefully designed to balance on-site with no import or export, totaling approximately 18,000 cubic yards of earthwork. Parking is tiered to reduce bulk and cut/fill impacts. A detention basin is located at the low point to manage stormwater runoff. Graded slopes will be stabilized with riprap or landscaping per geotechnical recommendations. No trees will be removed, and the site will be revegetated using a native upland seed mix with permanent irrigation.

Landscaping follows Washoe County Code Article 412 and includes drought-tolerant species and perimeter screening. A 6-7 ft tall solid vinyl earth-tone fence and evergreen trees every 20 linear feet buffer the site from adjacent residential uses.

Lighting complies with Washoe County standards using full cutoff LED fixtures at code-allowed pole heights. Lighting is concentrated near drive aisles and parking areas, avoiding spillover. Timers ensure lights operate only during active use hours, enhancing safety while minimizing impacts.

This project meets the intent of the LDS zoning and Development Code, ensuring a respectful, well-buffered, and high-quality quasi-public use within a residential context.



## Civil Engineering Plans

The civil design supports a responsible and respectful integration of a new place of worship into the neighborhood. Through balanced grading, controlled drainage, strategic layout, and naturalistic transitions, the project mitigates impacts on adjacent properties and promotes a compatible, stable, and aesthetically cohesive development in compliance with Washoe County's civil, grading, and drainage requirements. This approach allows the project to meet its functional needs while maintaining a natural, undisturbed edge and reducing visual and physical intrusion on neighboring homes. The subject site has moderate topographic variation, and the grading design has been carefully balanced to:

- Minimize excessive cut and fill, with earthwork limited to ~18,000 cubic yards and no import or export of material, reducing haul traffic and off-site disruption.
- Avoid harsh slope transitions by creating tiered parking areas, integrating the built form into the landscape and reducing the perceived mass from neighboring views.
- Maintain natural buffers and transition zones near the site perimeter, preserving existing vegetation or revegetating with native upland seed mix to soften visual impacts and provide a gradual edge between the development and surrounding properties.
- Retaining walls are not proposed, if determined necessary, will be limited in height and constructed using visually compatible materials to ensure a natural appearance.

### Drainage and Stormwater Management

All disturbed slopes will be stabilized with riprap or landscaping and revegetated using a standard upland native seed mix to prevent erosion and blend visually with the surrounding environment. To avoid off-site impacts from increased runoff, a detention basin is located at the site's low point near the main entry, adjacent to the primary drive aisle. This basin will:

- Capture and control peak stormwater flows in accordance with Washoe County and Truckee Meadows BMP standards.
- Be easily accessible for long-term maintenance and functionality.
- Prevent downstream flooding or erosion, protecting neighboring parcels from unintended runoff effects.

### Access and Circulation

The site provides controlled, single-entry access off Rolling Ridge Rd, with a secondary connection at Opal Station Drive to distribute vehicle circulation. Additionally, since no off-site grading is required and all construction is internal to the site, adjacent properties will not experience changes in grade or new topographic constraints.

- The drive aisles have been located to the interior of the site, away from residential property lines, minimizing vehicular noise and headlights affecting adjacent homes.
- The site layout ensures that fire access and turnarounds are accommodated without creating additional access roads along residential edges.
- ADA parking and general parking are distributed in tiers to reduce the extent of disturbance and reduce bulk.



### Utility Design and Coordination

- Water service will be established through annexation into Truckee Meadows Water Authority (TMWA), with new water rights purchased as required, ensuring no disruption to neighboring water systems.
- Sewer service will connect to existing municipal systems per Washoe County standards.
- All utilities are located internally to the site and routed to avoid shared easements or encroachments onto residential properties.

### Visual Mitigation and Neighborhood Transition

The proposed improvements are entirely within the property boundaries and are designed exclusively for this development; no shared access or grading with adjacent parcels is proposed.

Given the visibility of the site from all directions, the civil design includes several features to reduce the visual and environmental impact on neighboring residents:

- Landscaped perimeter buffers with native and drought-tolerant plantings.
- No grading in setback areas, preserving undisturbed edge vegetation wherever feasible.
- Full revegetation of disturbed slopes using an approved upland seed mix with permanent irrigation.
- Slopes are treated with landscaping and/or rockery to blend with the natural terrain and reduce visual contrast.
- No berms are proposed that would alter neighborhood viewsheds.

### Construction Considerations

No grading has been completed previously, and all disturbed areas are shown on the civil plans. During construction:

- Dust and erosion will be controlled using BMPs.
- Revegetation will be installed immediately after grading.
- The site will be monitored for slope stability and sediment control.



This **SUP** for grading is necessary due to site topography and design needs. The grading is limited to the minimum extent necessary, balanced on-site, and mitigated through engineering, landscaping, and visual screening measures. The proposal ensures compatibility with surrounding properties and compliance with Washoe County Code and the goals of the Master Plan. Pursuant to Washoe County Development Code Section 110.438.35 and Article 438 – Grading, a Special Use Permit is required because the proposed grading for this development exceeds the threshold limits set forth for disturbance and volume. Specifically:

- The project proposes approximately 18,000 cubic yards of earth movement, exceeding the 5,000 cubic yard threshold for unincorporated Washoe County residential zones.
- The total disturbed area exceeds one acre, further triggering the SUP requirement.
- The site design requires a combination of cut and fill to accommodate building pads, parking tiers, and drainage features on this sloped site.

### **Purpose and Intent of Grading**

The design avoids grading within setback areas and maintains undisturbed buffers where feasible to preserve compatibility with adjacent residential uses. The grading is necessary to:

- Establish a functional, code-compliant building pad for the proposed church facility.
- Construct tiered parking areas that reduce visual bulk and respect existing grades.
- Facilitate the construction of a detention basin for stormwater management at the natural low point of the site.
- Provide safe vehicular access and fire turnaround areas per TMFPD standards.

### **Mitigation of Grading Impacts**

The grading plan has been developed to minimize off-site impacts and meet the intent of Article 438:

- Balanced site: No import or export is proposed—earthwork is balanced to reduce truck traffic and environmental impacts.
- Slopes: All slopes are 2:1 or flatter and will be stabilized with riprap and/or native plantings, per geotechnical recommendations.
- Visual mitigation: Graded areas visible from off-site will be landscaped or revegetated using a native upland seed mix to blend with the natural environment.
- Drainage: A detention basin is included to manage runoff and protect adjacent properties from erosion or increased flow.
- No trees removed: The site contains only low-growing upland vegetation, and no tree removal is necessary.
- Retaining walls: If needed, retaining walls will be visually compatible with the residential character and will minimize cut and fill extents.



## 3485 ROLLING RIDGE RD

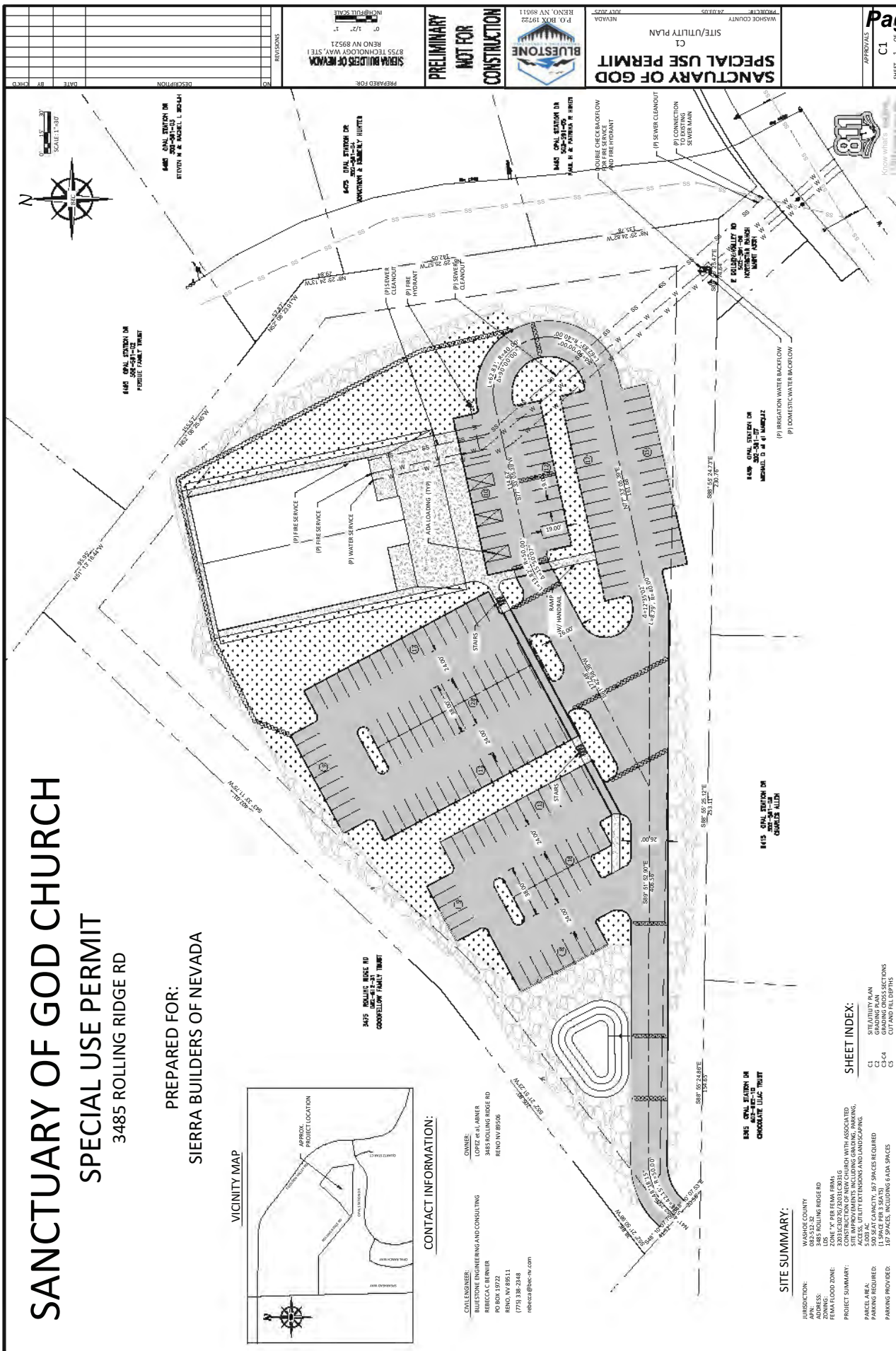
SIERRA BUILDERS OF NEVADA



OWNER:  
LOPEZ et al, ABNER  
3485 ROLLING RIDGE  
RENO NV 89506

**APN:** 082-512-32  
**AD ADDRESS:** 3485 ROLLING RIDGE  
**ZONING:** L0S  
**FEMA FLOOD ZONE:** ZONE "X" PER FEMA 32031C30270/32031C  
**PROJECT SUMMARY:** CONSTRUCTION OF NEW SITE IMPROVEMENTS: ACCESS, UTILITY EXTENSION, 5,003 AC, 500 SEAT CAPACITY, 1 SPACE PER 3 SEATS, 167 SPACES INCLUDED

C1	SITE/UTILITY PLAN
C2	GRADING PLAN
C3-C4	GRADING CROSS SECTIONS
C5	CUT AND FILL DEPTHS

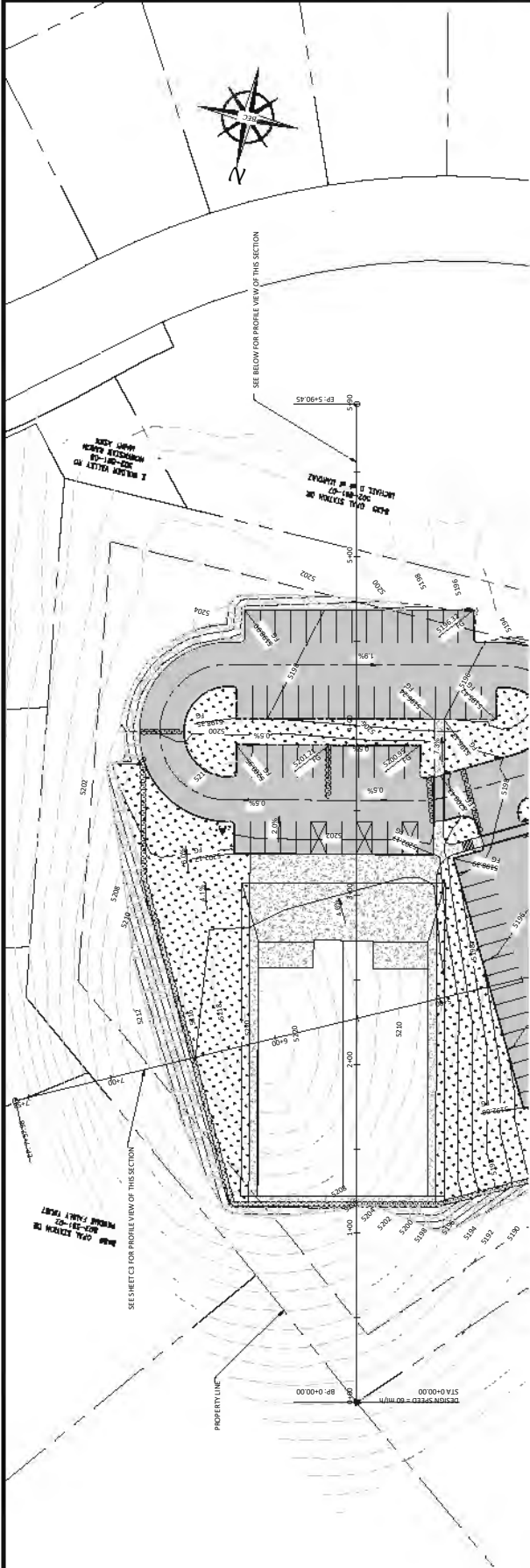






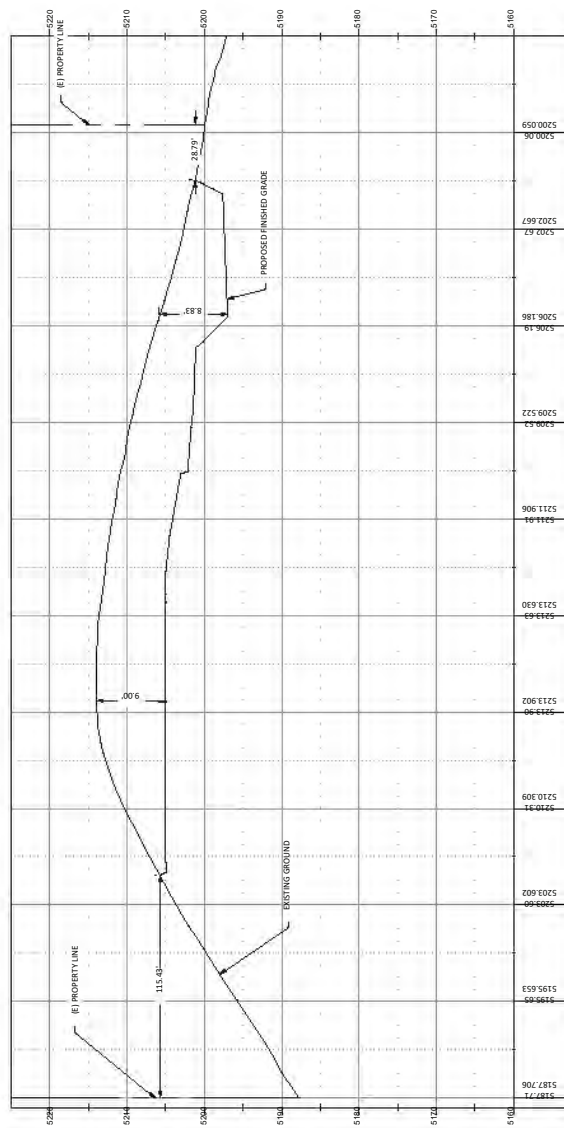


Page SHEET 4 OF 4 C4	APPROVALS	<div>SANCTUARY OF GOD SPECIAL USE PERMIT C4 CROSS SECTION NORTH - SOUTH WASHINGTON COUNTY NEVADA JULY 2025</div>	<div>BLUESTONE P.O. BOX 19722 RENO, NV 89511</div>	<div>CONSTRUCTION NOT FOR PRELIMINARY</div>	<div>PREPARED FOR: SERPA BUILDERS OF NEVADA 8755 TECHNOLOGE WAY, STE 1 RENO NV 89521 INCORPORATED 0" 1/2" = 1"</div>	REVISIONS	NO	DESCRIPTION	DATE	BY	CHKD



CROSS SECTION N-S

STA 0+05 TO STA 5+50









The proposed building is a single-story, 15,000 square foot church facility. The structure has been carefully designed to balance the spatial needs of a modern worship center with a scale and appearance that are compatible with adjacent residential uses. The proposed church facility is designed to serve the community while preserving the residential character of the neighborhood. Careful attention has been given to architectural scale, building materials, transparency, and site function. The maximum building height is limited to 23 feet at the ridge, well below typical height allowances in the zoning district. The building includes two primary functional areas:

- Approximately 9,000 square feet of Assembly (A-3) space to accommodate worship services. This area includes seating, a central stage, and arcade-style open circulation.
- Approximately 6,000 square feet of Business (B) support space, which includes offices, classrooms, restrooms, a breakroom/kitchenette, and a welcoming entrance foyer.

The building is classified as a Type VB, unseparated mixed-use structure, with clear delineation of the two primary occupancy types.

### Occupancy and Capacity

The estimated occupancy of the building is 531 persons, calculated in accordance with Table 1004.5 of the International Building Code. This occupancy count does not include general circulation areas such as the arcade, which are not classified as occupiable space under IBC standards.

The breakdown includes:

- Worship area (6,000 SF at 1 occupant per 15 SF): 400 occupants
- Foyer area (1,666 SF at 1 per 15 SF): 111 occupants
- Office and breakroom areas (2,500 SF at 1 per 150 SF): 17 occupants
- Restrooms and accessory spaces (900 SF at 1 per 300 SF): 3 occupants

### Architectural Design and Exterior Materials

The proposed architectural design emphasizes compatibility with the surrounding neighborhood and incorporates durable, attractive materials. The materials and design choices contribute to a subdued and welcoming architectural character, appropriate for a civic/religious use adjacent to residential properties. The exterior will feature:

- A stone wainscot base using cultured stone in natural earth tones/ visual grounding
- Painted architectural metal siding above the stone base /complementary neutral tones
- A dark standing seam metal roof, adding visual contrast and long-term weather resistance

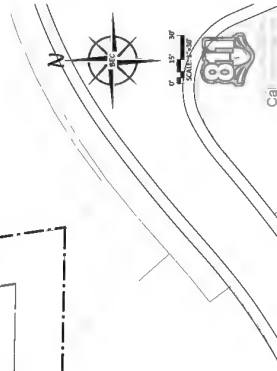
### Glazing and Transparency

Glazing is used intentionally to soften the building's appearance and establish a welcoming presence:

- The south (front) elevation includes a glass storefront system that designates the main entrance and creates visual transparency into the foyer area.
- East and west elevations feature windows serving office and classroom spaces.
- Overall glazing is used to break up wall planes, introduce light into interior spaces.

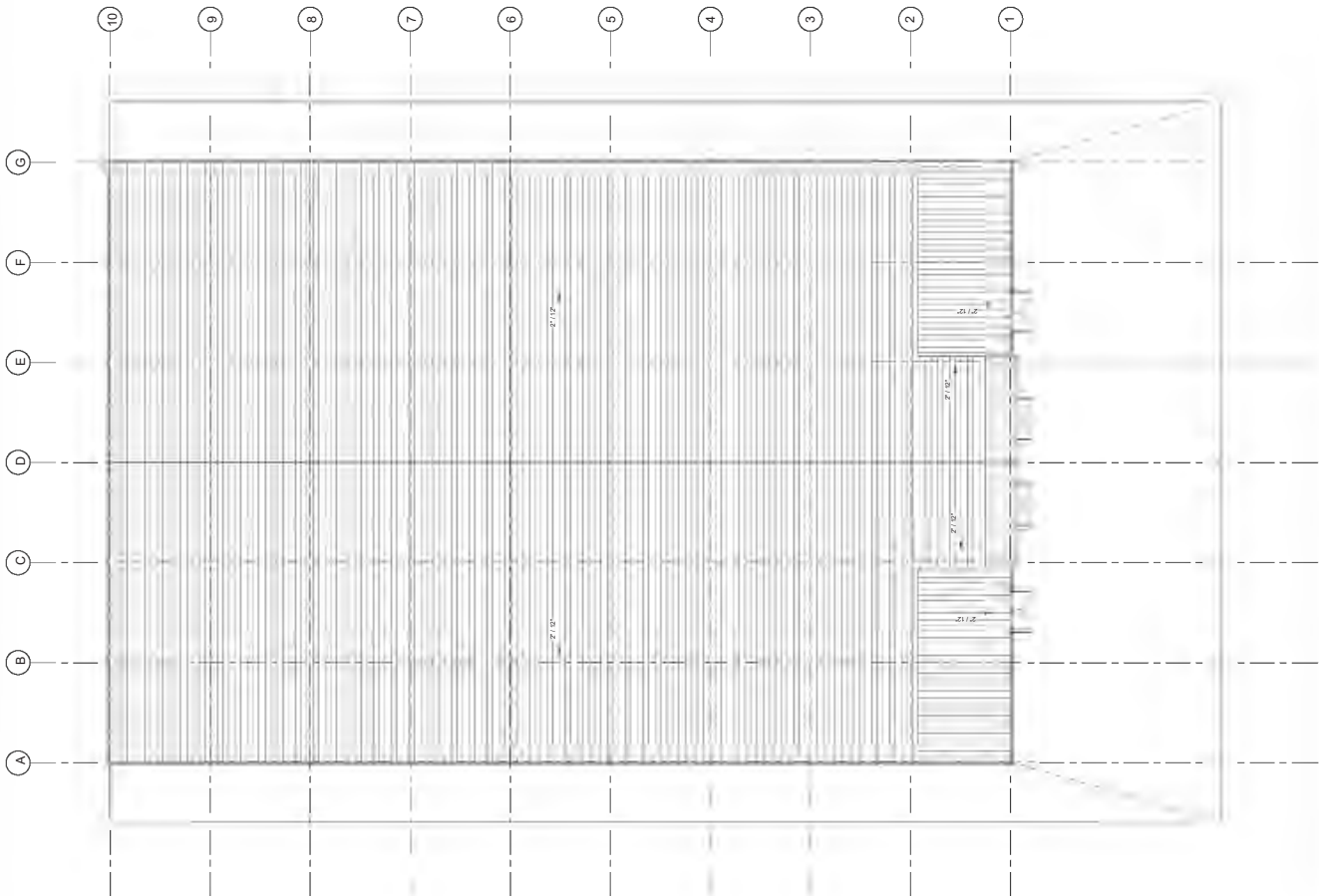
This approach will help the building read at a more residential scale. This reinforces compatibility with nearby homes and avoids the appearance of a large institutional structure.

- PARKING REQUIREMENTS:**
- 1. 100% of the total number of parking spaces shall be provided for the use of the building.
  - 2. The minimum number of parking spaces shall be determined by the number of employees and visitors.
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- PARKING PROVIDED:**
- 1. 100% of the total number of parking spaces shall be provided for the use of the building.
  - 2. The minimum number of parking spaces shall be determined by the number of employees and visitors.
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0" = 1" = 30'0"



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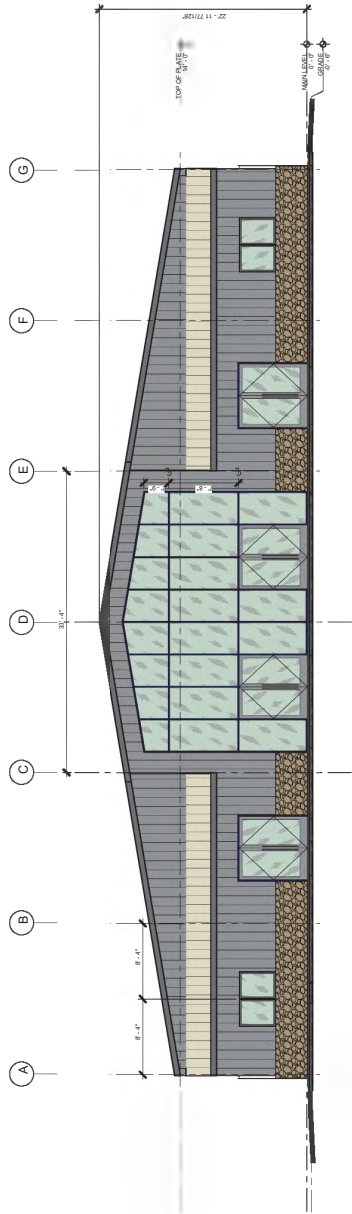
1 ROOF PLAN  
1/8" = 1'-0"



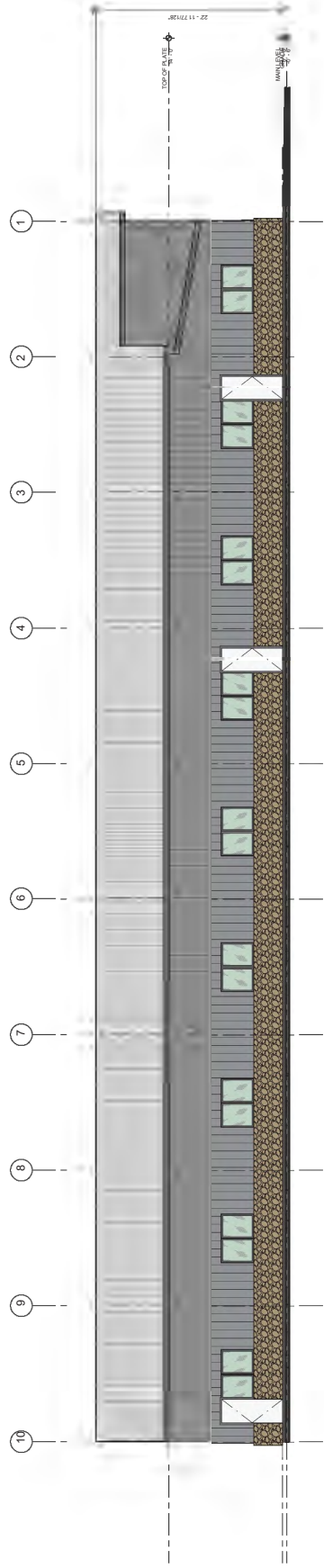




- KEY NOTES**
- 1. CONCRETE FOOTING, REEFER TO FOUNDATION PLAN FOR SIZE & REINFORCEMENT.
  - 2. CONCRETE FOUNDATION WALL, SEE STRUCTURAL FOR SIZE & REINFORCEMENT.
  - 3. ARCHITECTURAL NOTES GOVERN SPEC CLAUSE.
  - 4. EXTERIOR FINISH: STUCCO.
  - 5. DRINKING FOUNTAIN, ADA AND NORMAL HEIGHT.
  - 6. BREAK ROOM REFRIGERATOR, PER OWNER.
  - 7. BREAK ROOM SINK, PER OWNER.
  - 8. BREAK ROOM DISH WASH, PER OWNER.
  - 9. BREAK ROOM CASHIER, PER OWNER.
  - 10. LAV COUNTER TOP, ADA COMPLIANT, 3/4" MAX.
  - 11. LAV / SERVICE SINK / MOP SINK.
  - 12. WATER CLOSET, ADA COMPLIANT WHERE GRAB BARS SHOWN.
  - 13. URINAL.
  - 14. CLEAR ANODIZED ALUMINUM WINDOW FRAMES CAN DOUBLE GLAZED, VISION FILLED, 1/2" GLASS, 1/2" VISION FILLED.
  - 15. GLASS PARTITION, 1/2" GLASS, 1/2" VISION FILLED.
  - 16. MANUFACTURER, ELEGANT STONE.
  - 17. STILE, BY OWNER.
  - 18. 24 GA. PREPA. METAL STANDING SEAM ROOF, COLOUR: WHITE.
  - 19. 1/2" x 8" SP. 16" ON CENTER, STEEL BEAMS FROM PRE-ENG. SUPPLIER.
  - 20. 1/2" x 8" SP. 16" ON CENTER, STEEL BEAMS FROM PRE-ENG. SUPPLIER.
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  - 100. 1/2" x 8" SP. 16" ON CENTER, STEEL BEAMS FROM PRE-ENG. SUPPLIER.



1 SOUTH  
22' x 11' 7/8"



2 WEST  
22' x 11' 7/8"



## Landscape Plans

The conceptual landscape plan has been designed in full compliance with Washoe County Development Code, Chapter 110, Division 4, Article 412 – Landscaping, and considers both functional and aesthetic outcomes. The proposed plan reflects the intent to create a visually compatible, environmentally responsible, and community-oriented facility that integrates harmoniously with its natural and residential surroundings.

### **Plant Palette and Open Space Treatment**

All planting materials proposed for this project will be regionally appropriate, based on sound local horticultural practices, and selected for site suitability and water efficiency. The planting design has been intentionally developed to avoid traditional lawn or turf areas, emphasizing a xeriscape approach to reduce water consumption and minimize long-term maintenance.

### **Revegetation and Natural Open Space**

- Areas disturbed during construction, but not otherwise developed, will be revegetated using an Upland Seed Mix as recommended in the Truckee Meadows Construction Site BMP Handbook (2015), Appendix E. This mix includes native grasses and forbs selected for erosion control, quick establishment, and habitat compatibility.
- Undeveloped portions of the site, particularly those identified as open space buffers or natural area reserves, will remain in undisturbed upland vegetation, consisting primarily of native sagebrush scrub and other non-invasive species typical of the Northern Nevada high desert ecosystem. These areas provide natural stormwater infiltration, preserve the site's existing ecological character, and reduce the need for supplemental irrigation or long-term maintenance.

### **Irrigation and Water Management**

- All new planting areas will be supported by a fully automatic irrigation system using low-flow drip emitters, designed to comply with Truckee Meadows Water Authority (TMWA) guidelines.
- The irrigation system will include zoned controls for trees, shrubs, and groundcovers, and will be equipped with appropriate backflow prevention devices as required by code.
- Smart irrigation controllers with seasonal programming capabilities will be installed to optimize water use, prevent overwatering, and ensure the health of the installed plant material.



### Perimeter Treatments, Fencing, and Screening

To ensure compatibility with adjacent residential properties, the project includes a combination of structural fencing and vegetative screening, consistent with the standards for Civic and Commercial Use Types as defined in WCC 110.412.50(d) and 110.412.50(e).

#### Fencing

- A solid vinyl fence will be installed along the property perimeter where appropriate, with a minimum height of 6 feet and a maximum height of 7 feet, per code.
- The fence will be finished in a neutral earth-tone color, such as beige or brown, to visually blend with surrounding desert tones and adjacent development.

#### Vegetative Screening

- Along all property lines that abut residential uses, the landscape plan includes the installation of evergreen trees spaced at 20-foot intervals, placed in offset and staggered groupings. This pattern softens the overall appearance of the fence line and enhances screening.
  - Evergreen trees will be selected for height, density, and regional adaptability—potential species include Austrian pine, Bosnian pine, or similar drought-tolerant conifers.
  - Understory shrubs and groundcovers will be planted between trees and within buffer zones to provide layered vegetation, reinforce screening, and meet the required minimum plant coverage within the first two growing seasons.
- This approach ensures that visual, noise, and light impacts are minimized for neighboring properties while enhancing the aesthetic quality of the site's edge conditions.

#### Parking Lot Landscaping

The proposed site includes 154 off-street parking spaces. Parking lot landscaping has been designed to meet the intent and technical requirements of Washoe County Code, promoting shade, aesthetics, and environmental benefits.

- A minimum of one tree per ten parking spaces will be installed, resulting in at least 16 shade trees distributed throughout the parking area.
- Shade trees will be strategically located to reduce heat island effects, improve comfort for pedestrians, and break up the visual scale of the parking field.
- Peripheral parking lot landscaping will include low-water shrubs, ornamental grasses, and groundcovers that soften edges and reinforce circulation routes.

All parking lot plantings will be irrigated using the same efficient system described above, with drip lines routed to each tree and shrub group.

## **Landscape Plan**

### **Entry and Pedestrian Areas**

The landscape design highlights key pedestrian and entry points with accent planting and wayfinding elements to create a welcoming environment.

- The main building entrance will be framed by ornamental trees or flowering shrubs, such as serviceberry, Russian sage, or native lilac, to create a strong visual identity and seasonal interest.
- Pedestrian walkways will be lined with landscape planting and appropriately lit for visibility, safety, and aesthetics.
- All pedestrian areas are ADA accessible and will be designed to provide a comfortable transition between the parking area and the main facility.

### **Maintenance and Long-Term Compliance**

All plant materials will be maintained in a healthy, clean, and attractive condition throughout the life of the project. Compliance will be verified at time of final inspection and is subject to County code enforcement. Ongoing maintenance of all landscaped areas will be the responsibility of the property owner or operator. Maintenance activities will comply with WCC 110.412.45, and include:

- Seasonal inspections and adjustment of irrigation systems
- Replacement of any dead, diseased, or damaged plant material
- Routine pruning, weed removal, and litter control
- Maintenance of fence integrity and repainting or replacement of materials as needed

### **Signage**

All site signage—entry monument, directional, and building identification—will be carefully designed to be functional, compliant, and neighbor-friendly. The signs facilitate safe and efficient access while respecting Washoe County’s visual and lighting standards and enhancing the overall sense of place for this community facility.

#### **Entry Monument Sign**

- Location & Purpose: Strategically placed at the main entrance off Rolling Ridge Rd to identify the facility and direct visitors.
- Height & Size: Designed under the LDS district standards—max 8 feet in height, with total sign area within allowable limits.
- Materials & Finish: Durable, low-reflectivity materials in natural tones to complement architecture and landscape, providing a welcoming yet understated presence.





## **Landscape Plan**

### **Directional and Informational Signs**

- Placement: Installed throughout the site to guide vehicles and pedestrians safely to entry points, parking zones, and ADA-accessible routes.
- Type: Simple, non-illuminated signs with reflective lettering, conforming to Article 505 (i) standards for traffic control and public safety signage.
- Scale: Small-footprint, mounted on 3–4 foot posts, ensuring visibility without visual clutter in landscaped areas and drive aisles.

### **Building-Mounted Signage**

- Design: Modest wall-mounted sign above the main entry, sized to meet frontage-based allowances for quasi-public, non-commercial uses.
- Light and Glare: Non-illuminated or softly halo-lit to avoid glare; no spillover or skyward illumination ensures no adverse impacts on adjacent residences.

### **Code Compliance and Visual Mitigation**

- Article 505 Conformance: Sign size, placement, content, and materials meet all LDS zoning restrictions. No electronic message boards or flashing lights are proposed.
- Residential Compatibility: Signage is low-scale, neutral-toned, and visually integrated into landscape buffers—designed to complement, not compete with, nearby homes.
- Landscape Integration: Signs are surrounded by planting, groundcover, or mulch beds to soften edges and emphasize pedestrian scale.

Marc A.  
Chapelle PLA

Digitally signed by Marc A. Chapelle  
PLA  
DN: cn=Marc A. Chapelle PLA, o=L  
Studio Nevada LLC,  
email=marc@studionevada.com,  
c=US  
Date: 2023.07.07 16:13:48 -0700

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L.A. Studio Nevada  
the landscape architecture studio

1552 C Street  
Sparks, NV 89431 (775) 322-2223 NV PLA #440  
www.studiolandscape.com



Nevada

Preliminary Landscape Plan  
**SANCTUARY OF GOD**  
Sierra Builders

Washoe Co.

No. Revision Date

LA No: 630-007-06-08  
Designed: MAC  
Drawn: KSL  
Checked: RBL  
Date: 1/1/2008

Sheet: **L1** of 1

# PLANT LEGEND

- ORNAMENTAL TREES
- DECIDUOUS SHADE TREES
- EVERGREEN TREES
- LANDSCAPE AREA
- REVEGETATION AREAS

## LANDSCAPE DATA

SITE AREA: 217,031 SQ FT (5 ACRES)  
JURISDICTION: WASHOE COUNTY  
APN: 062 512 32  
ZONING: LDR - LOW DENSITY SUBURBAN  
DEVELOPED AREA: 161,172 SQ FT  
REQUIRED LANDSCAPE AREA OF DEVELOPED AREA = 32,235 SQ FT  
• (20% OF TOTAL SITE AREA)  
PROVIDED LANDSCAPE AREA = 32,235 SQ FT MIN.  
REQUIRED TREES = 124 MIN.  
• (1 TREE PER 300 SQ FT OF REQUIRED LANDSCAPE AREA) = 107  
• (1 TREE PER 10 PARKING SPACES - 107 SPACES PROVIDED) = 17  
• TOTAL TREES INCLUDES SCREENING TREES REQUIRED ALONG PERIMETER @ 1 TREE PER 20 LIN FT.  
ADJACENT TO EXISTING RESIDENTIAL (1,977 LIN. FT. / 20 = 99 TREES)  
REQUIRED SHRUBS = 744 MIN.  
• (8 SHRUBS PER REQUIRED TREE)

## GENERAL NOTES

- ALL PLANTING AND IRRIGATION SHALL BE INSTALLED PER LOCAL GOVERNING CODES.
- TREES
  - DECIDUOUS TREES SHALL HAVE A MINIMUM CALIPER OF 2 INCHES.
  - EVERGREEN TREES SHALL HAVE A MINIMUM HEIGHT OF 6 FEET
  - ADDITIONAL TREES, BEYOND THOSE REQUIRED BY CODE, MAY BE REDUCED IN SIZE AT INSTALLATION AND PROVIDED AT THE DISCRETION OF THE OWNER.
- FINAL PLANT SELECTION AND LAYOUT WILL BE BASED ON SOUND HORTICULTURAL PRACTICES RELATING TO MICRO-CLIMATE, SOIL, AND WATER REGIMES. ALL TREES WILL BE STAKED SO AS TO REMAIN UPRIGHT AND PLUMB FOLLOWING INSTALLATION. PLANT SIZE AND QUALITY AT TIME OF PLANTING WILL BE PER THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-1990).
- ALL SHRUB BEDS WILL RECEIVE 3" DEPTH MULCH WITH WEED CONTROL.
- ALL LANDSCAPING WILL BE AUTOMATICALLY IRRIGATED. CONTAINER PLANTINGS WILL BE DRIP IRRIGATED BASED ON THE SPECIFIC HORTICULTURAL REQUIREMENTS OF EACH SPECIES. A REDUCED-PRESSURE-TYPE BACKFLOW PREVENTER WILL BE PROVIDED ON THE IRRIGATION SYSTEM AS REQUIRED PER CODE.
- PLANTING PLAN IS CONCEPTUAL. PLANT QUANTITIES INDICATED ARE PER WASHOE COUNTY CODE REQUIREMENTS. PLANT LOCATIONS, FINAL SPECIES SELECTION, AND SIZE AT PLANTING SHALL BE DETERMINED DURING DEVELOPMENT OF FINAL CONSTRUCTION DOCUMENTS.
- AMENITIES AND AMENITY AREAS ARE CONCEPTUAL. FINAL SELECTION SHALL BE DETERMINED DURING DEVELOPMENT OF FINAL CONSTRUCTION DOCUMENTS.



EXISTING 15 FT. WIDE EQUESTRIAN TRAIL (OFF-SITE)  
ACCESS & FENCELINE (OFF-SITE)



SOLID VINYL FENCE  
COLOR: EARTHTONE  
MINIMUM HEIGHT: 72"  
MAXIMUM HEIGHT: 84"

0' 30' 60' 90'

Scale in Feet





## Lighting Plan

This lighting plan supports the operational needs of the church while preserving the quiet residential character of the area. The combination of shielded fixtures, reduced pole heights, setback-conscious placement, and automated control systems ensures full compliance with Washoe County lighting standards and demonstrates a responsible, neighbor-sensitive design. The proposed lighting plan for the place of worship has been carefully developed to meet all applicable standards of Washoe County Development Code Article 414. The exterior lighting is intended to:

- Enhance safety and visibility for pedestrians and vehicles
- Provide security lighting for the building, parking lot, and walkways.
- Minimize light trespass, glare, and visual impacts on adjacent residential properties.
- Maintain compliance with energy efficiency goals and dark sky principles.

### Code Compliance Summary – Article 414

The lighting plan complies with the following key provisions of Washoe County Code:

- WCC 110.414.10 – General Lighting Standards: Fixtures are fully shielded and mounted to direct light downward and prevent glare beyond property boundaries.
- WCC 110.414.20 – Commercial and Civic Use Standards: Applicable to quasi-public uses such as places of worship; lighting is placed to ensure uniformity in coverage and avoid over-illumination.
- WCC 110.414.25 – Light Trespass and Control: All lighting is full cutoff, designed to minimize spillover beyond the site boundary, particularly near residential zoning.

The subject property is zoned Low Density Suburban (LDS). According to WCC 110.414.20(c): “For nonresidential uses permitted in residential zones, such as quasi-public uses, pole-mounted lighting shall not exceed 15 feet in height when located within 100 feet of a residentially zoned parcel.” As such, all proposed pole lights:

- Are designed to be 12’ within 100 ft and 20’ outside of requirement, low poles are used near the perimeter where the site interfaces with residential uses.
- Are positioned within the interior of the site, away from residential property lines, primarily serving parking areas, drive aisles, and building entrances.

### Fixture Type and Operation

- Fixture Type: All lighting will be LED, full cutoff (zero uplight), and comply with the IESNA definition for full shielding.
- Timer-Controlled Operation: Exterior lights will be equipped with programmable timers to reduce or turn off lighting during non-operating hours, minimizing unnecessary nighttime illumination.
- Reduced Light in Setback Areas: No light poles or fixtures are located within the setback buffer adjacent to residential zones. Illumination is focused inward for both functional and aesthetic purposes.







Brooklyn Oswald  
Principal, Aryte Group  
boswald@arytegroup.com  
07.08.2025

To:  
Washoe County Planning and Building Division  
Community Services Department  
1001 E. Ninth Street  
Reno, NV 89512

RE: Request to Deem Application Complete – Neighborhood Meeting Timing  
Project: Special Use Permit – Place of Worship  
Location: 3485 Rolling Ridge Road, Washoe County, NV

Dear Planning Staff,

On behalf of the applicant, Aryte Group is submitting this letter regarding the neighborhood meeting requirement related to the Special Use Permit application for 3485 Rolling Ridge Road.

We understand that Washoe County requires a neighborhood meeting be conducted prior to the first public hearing and appreciate the County's commitment to early public involvement. While we fully intend to meet this requirement, we respectfully request that the application be deemed complete at this time for the following reasons:

- A neighborhood meeting has been scheduled for July 28 at North Valleys High School, which will take place several weeks ahead of any anticipated Board of Adjustment hearing. This provides ample time for meaningful public input, discussion, and incorporation of potential project modifications or conditions.

- A courtesy notice has already been mailed to property owners within 500 feet of the project site upon submittal, consistent with County practice. The notice includes project details and applicant contact information to encourage early feedback from the community.

- The County Advisory Board (CAB), which previously served as a venue for initial public comment, is no longer active for this area. The planned neighborhood meeting fills that public engagement role.

While we understand and respect the County's policy, we believe that requiring the neighborhood meeting to be conducted before deeming the application complete may be inconsistent with timelines established under Nevada Revised Statutes (NRS) related to development application processing. Specifically, NRS requires that applications be reviewed within specified timeframes once submitted, and adding pre-submittal conditions not codified in the Development Code could raise procedural concerns.

Therefore, we respectfully request that the application be deemed complete at this time, with the neighborhood meeting to occur prior to the public hearing in accordance with Washoe County Code. This approach maintains compliance with both local code and state law while ensuring robust public engagement.

We appreciate your time and consideration and look forward to working collaboratively through the remainder of the review process.

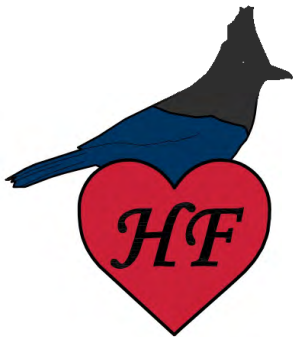
Sincerely,  
Brooklyn Oswald  
Principal, Aryte Group



# Trip Generation Study

Project: Sanctuary of God Church  
3485 Rolling Ridge Rd  
Reno, NV 89506  
Washoe County APN 082-512-32  
File No: 235.001

Prepared by:



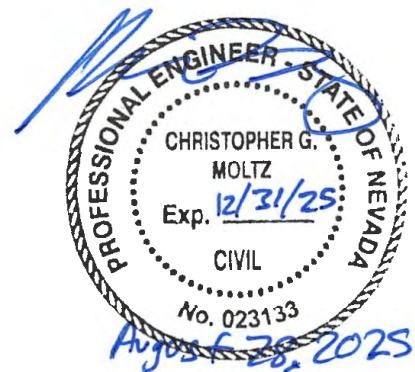
## Heartfelt Engineering, LLC

PO Box 2457  
Carson City, Nevada 89702  
[chris@heartfeltengineering.com](mailto:chris@heartfeltengineering.com)  
775-546-5582

August 28, 2025

Prepared for:

Sanctuary of God Church for submittal to Washoe County,  
Nevada



## **Executive Summary**

This report is to serve as a Trip Generation Study to adequately show that a full Traffic Impact Study is not required for this project. Per Washoe County requirements, Traffic Impact Studies are required whenever the proposed development project will generate 80 or more peak hour trips as determined using the latest edition Institute of Transportation Engineers (ITE) trip generation rates.

The proposed project (Sanctuary of God Church) is the installation of a new church to be located at 3485 Rolling Ridge Road in Reno, Nevada (rural Washoe County) on Washoe County APN 082-512-32 (located at the end of Rolling Ridge Road). The project consists of an approximate 15,000 square foot building; however, the majority of the building will be dedicated for uses other than the church (storage, private offices, a conference room, etc). Only 6,498.52 square feet is dedicated to church service activities (Service and Stage Area). The church currently meets at other locations, and sees average attendance between 85 and 95 attendees (excluding ten volunteer staff including the pastor and band who arrive onsite in the hour prior to the first service). Church services are held on Sunday mornings, and the other land uses (office / storage / conference room, etc) will only be used outside of church service times (during the weekday, etc). ITE Land Use 560: Church was identified as an accurate land use for this project. Weekday AM and PM Peak Hour Trips, Saturday Peak Hour Trips, and Sunday Peak Hour Trips were reviewed for ITE Land Use 560: Church. As can be expected, Sunday Peak Hour trips for the proposed church use are significantly higher than any other days or times. Based on review of published ITE data, 67.32 peak hour trips can be expected during the Sunday Peak Hour of Generator. This is below the 80 peak hour trip threshold for a Traffic Impact Study. Additionally, if we go by the maximum 95 average attendees, this comes to an estimated 50.35 peak hour trips which can be expected during the Sunday Peak Hour of Generator. This is also below the 80 peak hour trip threshold for a Traffic Impact Study. Reference Attachment 4 of this report for cited ITE Data.

Traffic generated by this new project will have a negligible impact on the adjacent street network, particularly as the peak hours of traffic will occur on Sunday morning.

No changes to the roadway network are required for this project aside from the proposed paved roadway approach off Rolling Ridge Road.

North Valleys High School was identified approximately half a mile west of the project location; however, the need for additional traffic control devices was not identified as a result of this project.

**Due to the low number of trips (less than 80 peak hour trips) estimated to be generated by this project, the need for additional traffic analysis is not required per Washoe County Requirements.**

Documentation identifying these conditions and the analysis to come to these conclusions have been included in this report. All work and construction shall be done in compliance with any applicable Washoe County standards (as applicable).

## **I. Introduction**

The proposed project (Sanctuary of God Church) is the installation of a new church to be located at 3485 Rolling Ridge Road in Reno, Nevada (rural Washoe County) on Washoe County APN 082-512-32 (located at the end of Rolling Ridge Road). All access to the property will be from Rolling Ridge Road (a local dead end road), which intersects with Spearhead Way (another local road) approximately a quarter mile west of the project location. The majority of Traffic is anticipated to access the property from E. Golden Valley Road, located north and east of the property via Spearhead Way to Rolling Ridge Road to the project. E. Golden Valley Road is classified by NDOT as a "Minor Arterial" in the vicinity of the project location. Rolling Ridge Road is controlled by a one-way stop control, leaving Spearhead Way traffic uninterrupted, and Spearhead Way traffic is controlled by two-way stop controls, leaving E. Golden Valley Road traffic uninterrupted. The total driving distance from the project driveway to E. Golden Valley Road is approximately 0.42 miles. The project is located within Washoe County, approximately 1.5 miles east of US 395 and the Golden Valley Road Ramps, and the entrance of the property is approximately 0.2 miles west and south of E. Golden Valley Road. The property is located in the N½ NE¼ of Section 14, T.20N., R.19E., M.D.B.&M. The purpose of this report is to determine if a sufficient number of trips will be generated by this project, and if enough trips will be generated to warrant a traffic impact study.

The project consists of an approximate 15,000 square foot building; however, the majority of the building will be dedicated for uses other than the church (storage, private offices, a conference room, etc). Only 6,498.52 square feet is dedicated to church service activities (Service and Stage Area). The church currently meets at other locations, and sees average attendance between 85 and 95 attendees (excluding ten volunteer staff including the pastor and band who arrive onsite in the hour prior to the first service). Church services are held on Sunday mornings, and the other land uses (office / storage / conference room, etc) will only be used outside of church service times (during the weekday, etc). The church is only staffed by volunteers, so use will be very low during regular weekday hours. Therefore, only the floor area associated with Sunday church service areas is the previously identified 6,498.52 square foot area.

Access currently exists for the property in the form of a dead end bulb / fire turnaround located at the east end of Rolling Ridge Road. The proposed access road / driveway approach is a paved approach to be installed at the end of the dead end bulb / fire turnaround. The property is surrounded other residential properties. A general location map utilizing Google Earth aerials is included as Attachment 1. A project location map utilizing aerials from the Washoe Regional Mapping System is included as Attachment 2. A Conceptual Site Layout / Building Plans (provided by Sierra Builders of Nevada) for the proposed project is included in Attachment 3.

## **II. Existing Roadways and Intersections and Site Conditions**

The vicinity of the project location includes the local streets of Rolling Ridge Road running east/west (of which the proposed project is at the far east end of), Spearhead Way (running north/south), and East Golden Valley Road which is the Minor Arterial for the area (running east/west). I am familiar with this area and the existing one-way and two-way stop controls appear sufficient for controlling traffic for the existing roadway intersections, and this project.

Traffic in this area has been identified as extremely light along Rolling Ridge Road, with higher volumes of traffic along East Golden Valley Road (particularly during weekday school drop-off times and rush hour). The speed limit for Rolling Ridge Road is identified as 25 mph.

No local warrants such as school zones have been identified which warrant the need for any additional traffic considerations or controls. North Valleys High School is located 0.5 miles west of the project location in a straight line, however, it is sufficient distance from the church, and the church will not have high volumes of traffic on school days (Sunday is the busy day for the church). Therefore, this project is not affected by the high school or vice versa.

Two-way access to the site will be via the proposed paved approach driveway at the east end of Rolling Ridge Road, approximately a quarter mile east of the intersection of Rolling Ridge Road and Spearhead Way. The new paved approach appears to be in the best (and only) possible location. No changes to the roadway network are required for this project aside from the proposed paved roadway approach off Rolling Ridge Road

## **III. Engineering Analysis to Determine if a Traffic Study is Required**

Per Washoe County requirements (County Code 110.436), Traffic Impact Studies are required whenever the proposed development project will generate 80 or more peak hour trips as determined using the latest edition Institute of Transportation Engineers (ITE) trip generation rates.

These conditions were reviewed to determine if a Traffic impact report was required for the proposed project. Here are the following conditions and the resultant findings:

### **1. 80 or More Peak Hours Trips**

ITE Land Use 560: Church was identified as an accurate land use for this project. Weekday AM and PM Peak Hour Trips, Saturday Peak Hour Trips, and Sunday Peak Hour Trips were reviewed for ITE Land Use 560: Church. As can be expected, Sunday Peak Hour trips for the proposed church use are significantly higher than any other days or times.

As previously identified, the project consists of an approximate 15,000 square foot building; however, the majority of the building will be dedicated for uses other

than the church (storage, private offices, a conference room, etc). Only 6,498.52 square feet is dedicated to church service activities (Service and Stage Area). The church currently meets at other locations, and sees average attendance between 85 and 95 attendees (excluding ten volunteer staff including the pastor and band who arrive onsite in the hour prior to the first service). Church services are held on Sunday mornings, and the other land uses (office / storage / conference room, etc) are only used outside of church service times (during the weekday, etc). The church is only staffed by volunteers, so use will be very low during regular weekday hours. Therefore, only the floor area associated with Sunday church service areas is the previously identified 6,498.52 square foot area.

Based on review of published ITE data, an average of 10.36 trips can be expected for every 1,000 square feet of church floor area. Based on the 6,498.52 square foot church service area, 67.32 peak hour trips can be expected during the Sunday Peak Hour of Generator. This is below the 80 peak hour trip threshold for a Traffic Impact Study. Additionally, if we go by the maximum 95 average attendees, this comes to an estimated 50.35 peak hour trips which can be expected during the Sunday Peak Hour of Generator. This is also below the 80 peak hour trip threshold for a Traffic Impact Study. Reference Attachment 4 of this report for cited ITE Data.

#### **IV. Conclusions:**

Per Washoe County requirements (County Code 110.436), Traffic Impact Studies are required whenever the proposed development project will generate 80 or more peak hour trips as determined using the latest edition Institute of Transportation Engineers (ITE) trip generation rates.

The proposed project (Sanctuary of God Church) is the installation of a new church to be located at 3485 Rolling Ridge Road in Reno, Nevada (rural Washoe County) on Washoe County APN 082-512-32 (located at the end of Rolling Ridge Road). The project consists of an approximate 15,000 square foot building; however, the majority of the building will be dedicated for uses other than the church (storage, private offices, a conference room, etc). Only 6,498.52 square feet is dedicated to church service activities (Service and Stage Area). The church currently meets at other locations, and sees average attendance between 85 and 95 attendees (excluding ten volunteer staff including the pastor and band who arrive onsite in the hour prior to the first service). Church services are held on Sunday mornings, and the other land uses (office / storage / conference room, etc) will only be used outside of church service times (during the weekday, etc). ITE Land Use 560: Church was identified as an accurate land use for this project. Weekday AM and PM Peak Hour Trips, Saturday Peak Hour Trips, and Sunday Peak Hour Trips were reviewed for ITE Land Use 560: Church. As can be expected, Sunday Peak Hour trips for the proposed church use are significantly higher than any other days or times. Based on review of published ITE data, 67.32 peak hour trips can be expected during the Sunday Peak Hour of Generator. This is below the 80 peak hour trip threshold for a Traffic Impact Study. Additionally, if we go by the



maximum 95 average attendees, this comes to an estimated 50.35 peak hour trips which can be expected during the Sunday Peak Hour of Generator. This is also below the 80 peak hour trip threshold for a Traffic Impact Study. Reference Attachment 4 of this report for cited ITE Data.

Traffic generated by this new project will have a negligible impact on the adjacent street network, particularly as the peak hours of traffic will occur on Sunday morning.

No changes to the roadway network are required for this project aside from the proposed paved roadway approach off Rolling Ridge Road.

North Valleys High School was identified approximately half a mile west of the project location; however, the need for additional traffic control devices was not identified as a result of this project.

**Due to the low number of trips (less than 80 peak hour trips) estimated to be generated by this project, the need for additional traffic analysis is not required per Washoe County Requirements.**

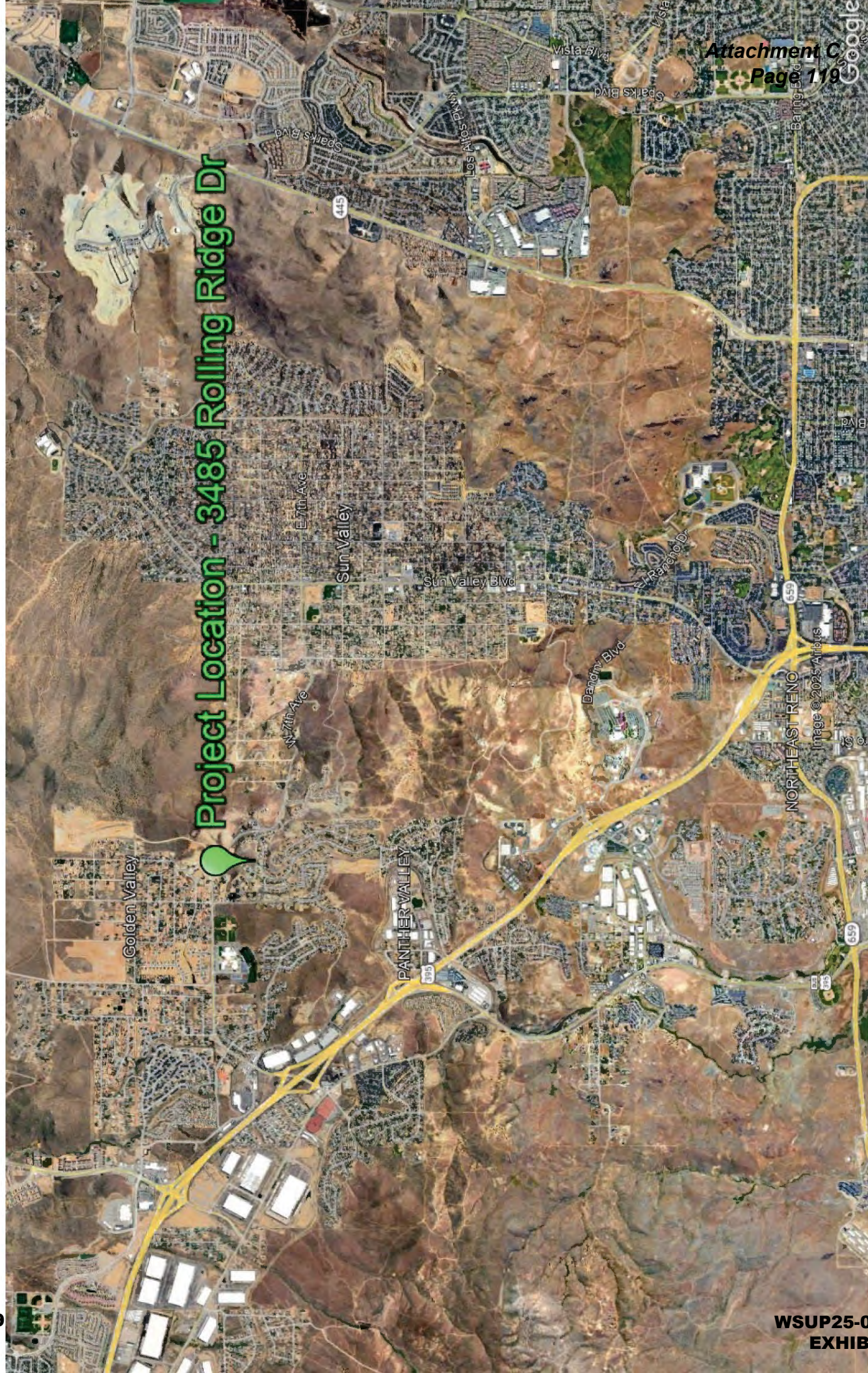
Documentation identifying these conditions and the analysis to come to these conclusions have been included in this report.

All work and construction shall be done in compliance with any applicable Washoe County standards (as applicable).

## **Attachment 1 – General Location Map**

(aerials from Google Earth Imagery Software)







## **Attachment 2 – Project Location Map**

(aerials from Washoe County Regional Mapping System)











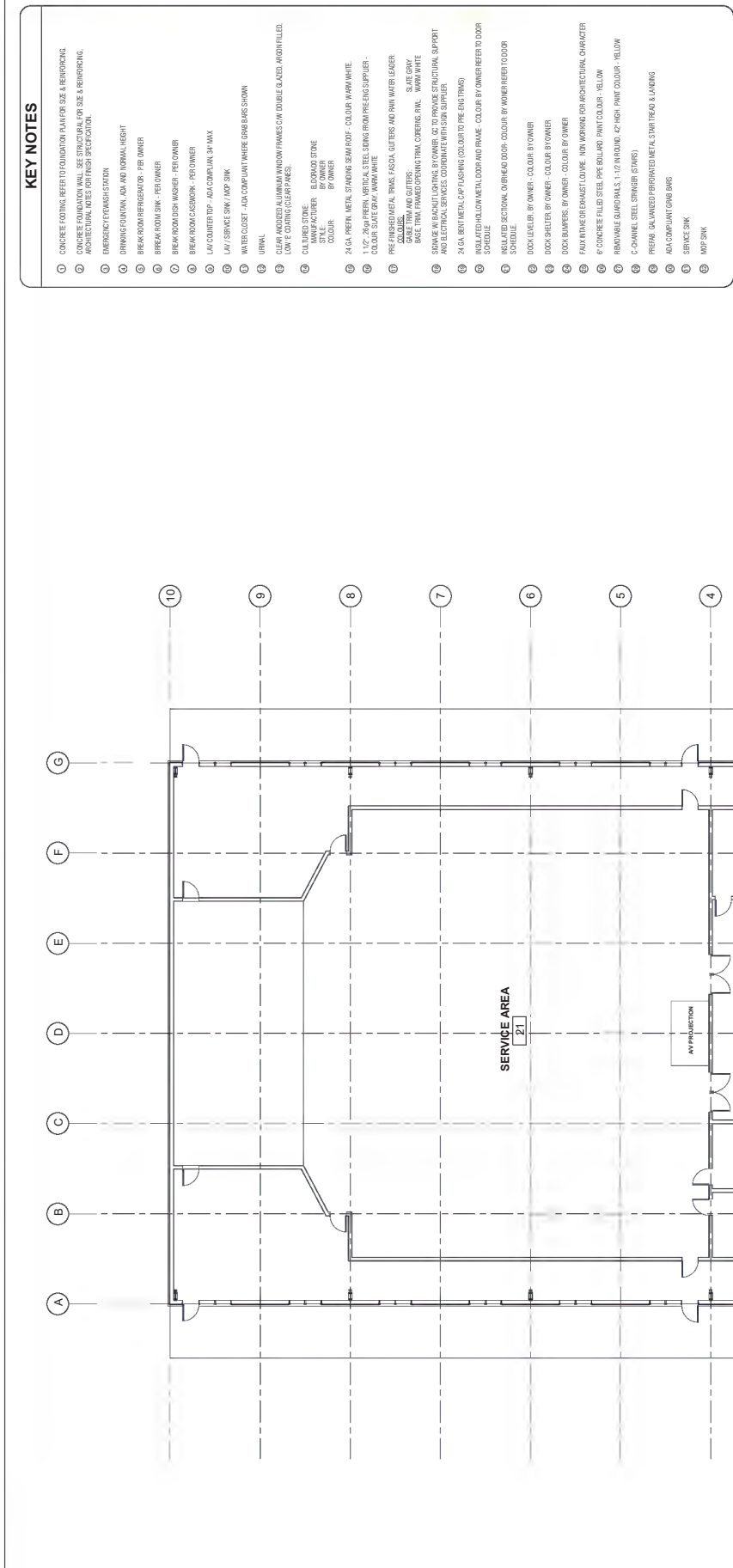
## **Attachment 3 – Conceptual Site and Building Plans**

(provided by Sierra Builders of Nevada)



○ SITE  
1" = 30.0'

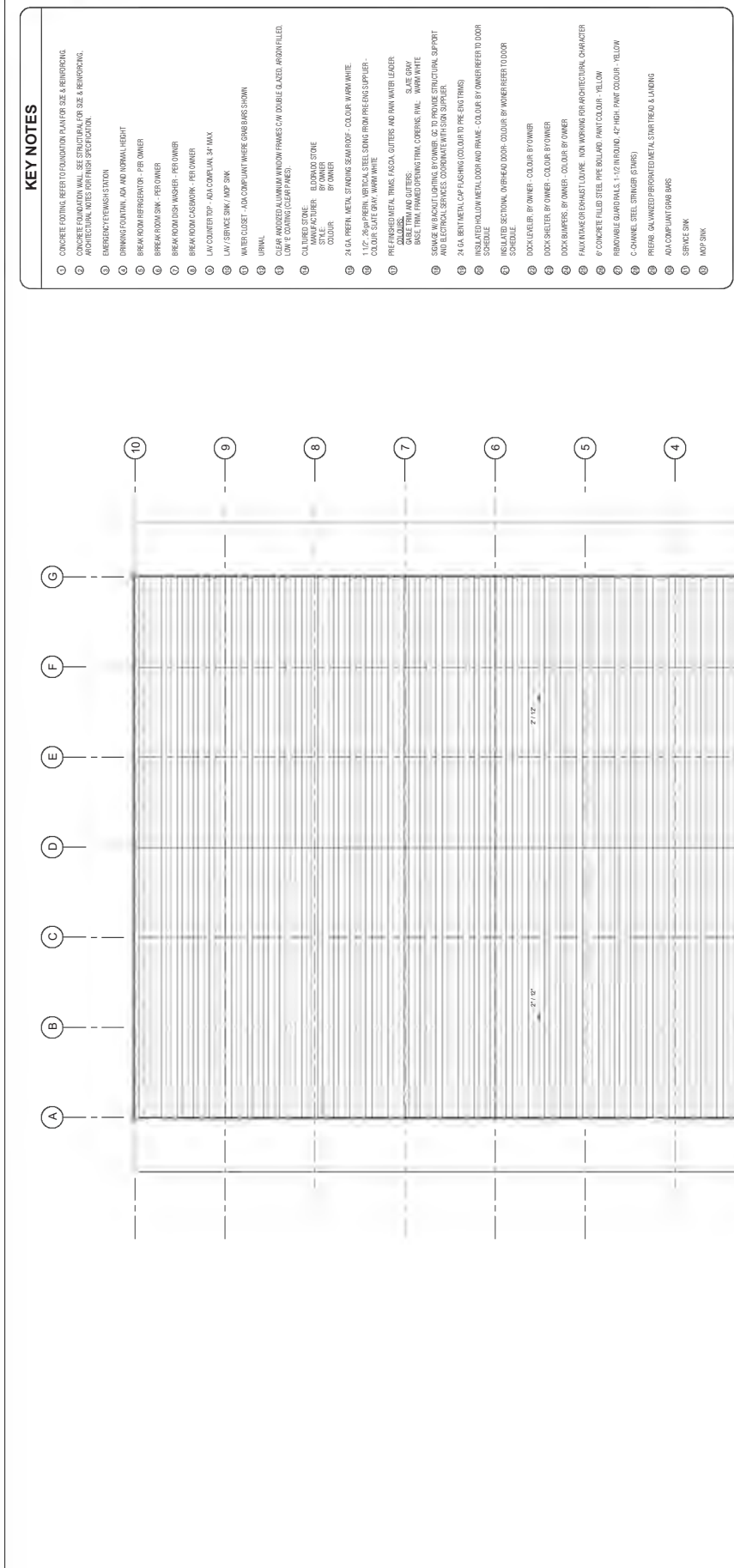
- PARKING REQUIREMENTS**
- 1. 100% of the required parking spaces shall be provided on-site.
  - 2. 100% of the required parking spaces shall be provided on-site.
  - 3. 100% of the required parking spaces shall be provided on-site.
  - 4. 100% of the required parking spaces shall be provided on-site.
  - 5. 100% of the required parking spaces shall be provided on-site.
  - 6. 100% of the required parking spaces shall be provided on-site.
  - 7. 100% of the required parking spaces shall be provided on-site.
  - 8. 100% of the required parking spaces shall be provided on-site.
  - 9. 100% of the required parking spaces shall be provided on-site.
  - 10. 100% of the required parking spaces shall be provided on-site.
- PARKING PROVIDED**
- 1. 100% of the required parking spaces shall be provided on-site.
  - 2. 100% of the required parking spaces shall be provided on-site.
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  - 10. 100% of the required parking spaces shall be provided on-site.



KEY NOTES

- 1 CONCRETE FOOTING REFER TO FOUNDATION PLAN FOR SIZE & REINFORCING
- 2 CONCRETE FOUNDATION WALL SEE STRUCTURAL FOR SIZE & REINFORCING
- 3 ARCHITECTURAL NOTES FOR FINISH SPECIFICATION
- 4 EMERGENCY EXIT MARKING
- 5 DRINKING FOUNTAIN, ADA AND NORMAL HEIGHT
- 6 BREAK ROOM REFRIGERATOR - PER OWNER
- 7 BREAK ROOM SINK - PER OWNER
- 8 BREAK ROOM DISH WASHER - PER OWNER
- 9 BREAK ROOM CASHIERS - PER OWNER
- 10 LAV COUNTER TOP - ADA COMPLIANT 34" MAX
- 11 LAV / SERVICE SINK / MOP SINK
- 12 WATER CLOSET - ADA COMPLIANT WHERE GRAB BARS SHOWN
- 13 URINAL
- 14 CLEAR ANCHORED ALUMINUM WINDOW FRAMES IN DOUBLE GLAZED, REINFORCED, LOW E GLAZING (CLEAR PANE)
- 15 CULTURED STONE
- 16 ELEVATOR STONE
- 17 ELEVATOR STONE
- 18 ELEVATOR STONE
- 19 24 GA. METAL STANDING SEAM ROOF - COLOUR: WHITE
- 20 1 1/2" DIA. VERTICAL VENTILATION STEEL LINING FROM PRELIMS SUPPLIER - COLOUR: WHITE
- 21 PRE-PANCHED METAL TRIMS / FASCIA, GUTTERS AND DOWN WATER LEADER
- 22 GABLE TRIM AND GUTTERS - COLOUR: WHITE
- 23 BASE TRIM, FINISHED OPENING TRIM, CORNER, FINL - WHITE
- 24 SCHEME IN EACH UTILITY ROOM, BY OWNER, GO TO PROVIDE STRUCTURAL SUPPORT AND ELECTRICAL SERVICES SUBSTANTIAL WITH FINISH SURFACES
- 25 24 GA. BENT METAL CAP FLASHING (COLOUR TO PRELIMS TRIMS)
- 26 INSULATED-HOLLOW METAL DOOR AND FRAME - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 27 INSULATED SECTION, OVERHEAD DOOR - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 28 DOCK LEVELER - BY OWNER - COLOUR BY OWNER
- 29 DOCK SHUTTER - BY OWNER - COLOUR BY OWNER
- 30 DOCK BUMPERS - BY OWNER - COLOUR BY OWNER
- 31 PAINT NAME ORIGINAL COLOUR - FOR WORKING FOR ARCHITECTURAL CHARACTER
- 32 6" CONCRETE FILLER STEEL PIPE GALVALD / PAINT COLOUR - YELLOW
- 33 REMOVABLE GUARDRAILS, 1 1/2" IN ROUND, 42" HIGH PAINT COLOUR - YELLOW
- 34 C CHANNEL STEEL STRINGER (STAIRS)
- 35 PREPARE GALVANIZED PERFORATED METAL START TREADS & LANDING
- 36 ADA COMPLIANT GRAB BARS
- 37 SERVICE SINK
- 38 MOP SINK



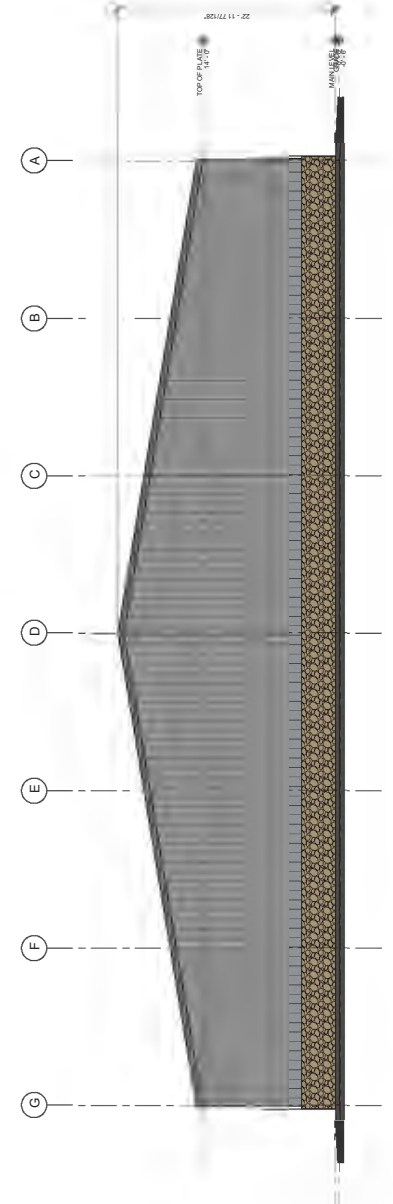


KEY NOTES

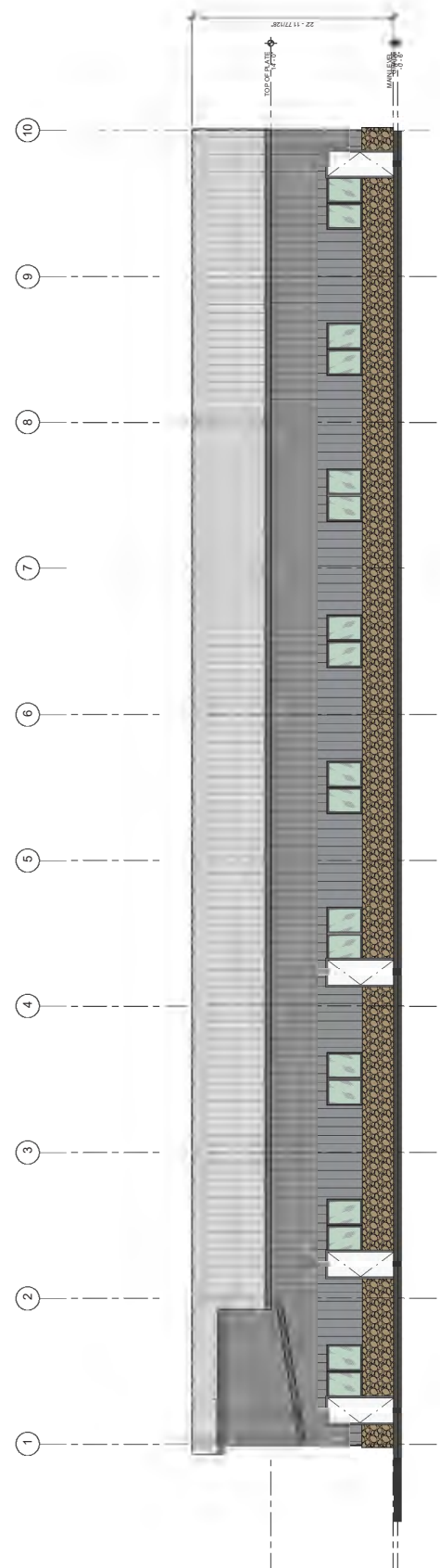
- 1. CONCRETE FOOTING, REFER TO FOUNDATION PLAN FOR SIZE & REINFORCING
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- 3. EMERGENCY EGRESS
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- 5. BREAK ROOM REFRIGERATOR - PER OWNER
- 6. BREAK ROOM SINK - PER OWNER
- 7. BREAK ROOM DISH WASHER - PER OWNER
- 8. BREAK ROOM CASHIERS - PER OWNER
- 9. LAUNDRY COUNTER TOP - ADA COMPLIANT 34" MAX
- 10. LAUNDRY SERVICE SINK - 18" SINK
- 11. WATER CLOSET - ADA COMPLIANT WHERE GRAB BARS SHOWN
- 12. URINAL
- 13. CLEAR WOODEN ALUMINUM WINDOW FRAMES C/W DOUBLE GLAZED, REINFORCED, LOW E GLAZING (CLEAR PAGES)
- 14. CULTURED STONE
- 15. MANUFACTURER
- 16. 24 GA. METAL STANDING SEAM ROOF - COLOUR: WHITE
- 17. 1/2" 304 SS VERTICAL STEEL SLUING FROM PRELIMS SUPPLIER - COLOUR: WHITE
- 18. PREPARED METAL TRIMS, FASCIA, GUTTERS AND DOWN WATER LEADERS
- 19. GABLE TRIM, FINISHED OPENING TRIM, CORNER, ETC. - 3/4" 304 SS
- 20. SCHEME IN EACH UTILITY ROOM, REFER TO PROVIDE STRUCTURAL SUPPORT AND ELECTRICAL SERVICES SUBSTANTIAL WITH OWNERS SUPPLIER
- 21. 24 GA. BENT METAL CAPPING FLASHING (COLOUR TO PRELIMS TRIMS)
- 22. INSULATED/ALUMINUM METAL DOOR AND FRAME - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 23. INSULATED SECTION, OVERHEAD DOOR - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 24. DOCK LEVELER - BY OWNER - COLOUR BY OWNER
- 25. DOCK SHUTTER - BY OWNER - COLOUR BY OWNER
- 26. DOCK BUMPERS - BY OWNER - COLOUR BY OWNER
- 27. PAINT NAME ORIGINAL COLOUR - NON WORKING FOR ARCHITECTURAL CHARACTER
- 28. 6" CONCRETE FILLER STEEL PIPE COLLARD PAINT COLOUR - YELLOW
- 29. REMOVABLE GUARDRAILS, 1-1/2" IN ROUND, 42" HIGH PAINT COLOUR - YELLOW
- 30. C CHANNEL STEEL STRINGER (STAIRS)
- 31. PREPARED GALVANIZED PERFORATED METAL START TREADS & LANDINGS
- 32. ADA COMPLIANT GRAB BARS
- 33. SERVICE SINK
- 34. MOP SINK

KEY NOTES

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- 3 EMERGENCY TREX/SHEDDING
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- 5 BREAK ROOM REFRIGERATOR - PER OWNER
- 6 BREAK ROOM SINK - PER OWNER
- 7 BREAK ROOM DISH WASHER - PER OWNER
- 8 BREAK ROOM CASHIERS - PER OWNER
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- 15 EXTERIOR STONE
- 16 24 GA. METAL STANDING SEAM ROOF - COLOUR: WARM WHITE
- 17 1/2" X 20 GA. METAL VERTICAL STEEL SLUING FROM PRELIMS SUPPLIER - COLOUR: WARM WHITE
- 18 PREPARED METAL TRIMS, FASCIA, GUTTERS AND DOWN WATER LEADERS - COLOUR: WARM WHITE
- 19 GABLE TRIM, PAVED OPENING TRIM, CORNERING, FINL - WARM WHITE
- 20 SINKAGE IN BACKSLUSHING BY OWNER, GC TO PROVIDE STRUCTURAL SUPPORT AND ELECTRICAL SERVICES SUBSISTANT WITH GC'S SUPPORTER
- 21 24 GA. BENT METAL CAP FLASHING (COLOUR TO PRE ENG TRIMS)
- 22 INSULATED ALUMINUM METAL DOOR AND FRAME - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 23 ISOLATED SECTIONAL OVERHEAD DOOR - COLOUR BY OWNER REFER TO DOOR SCHEDULE
- 24 DOCK LEVELER - BY OWNER - COLOUR BY OWNER
- 25 DOCK SHUTTER - BY OWNER - COLOUR BY OWNER
- 26 DOCK BUMPERS - BY OWNER - COLOUR BY OWNER
- 27 PAINT NAME OR SIGNAGE TO CLIMATE - NON WORKING FOR ARCHITECTURAL CHARACTER
- 28 6" CONCRETE FILLED STEEL PIPE BOLLARD PAINT COLOUR - YELLOW
- 29 REMOVABLE GUARDRAILS, 1-1/2" IN ROUND, 42" HIGH PAINT COLOUR - YELLOW
- 30 C-DRAWN STEEL SPRINGER (STAIRS)
- 31 PRESS. GALVANIZED PERFORATED METAL START TREAD & LANDING
- 32 ADA COMPLIANT GRAB BARS
- 33 SERVICE SINK
- 34 MOP SINK



1 NORTH  
1" = 10'



2 EAST  
1" = 10'



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2025\_35

SCHEMATIC DESIGN

DATE: 08/20/2025

BY: KRI

FOR: WSUP25-0013

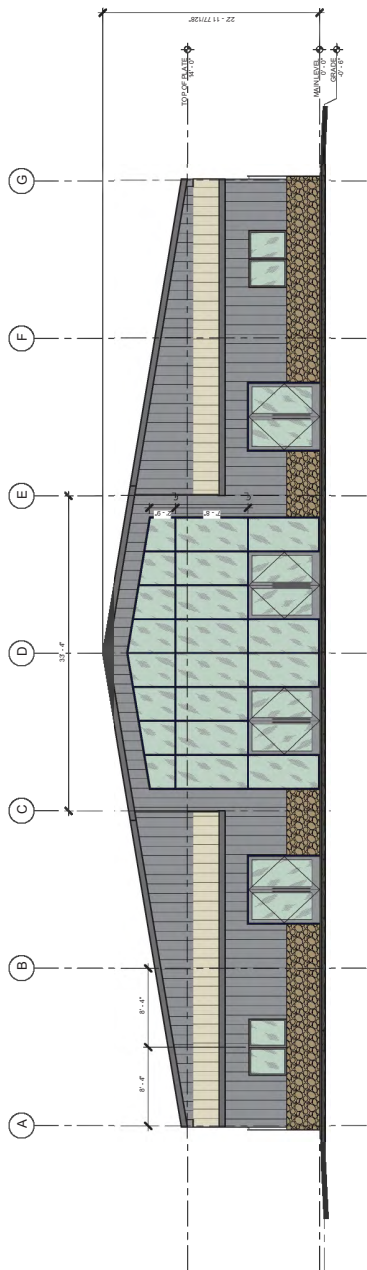
Attachment C  
Page 128

ELEVATIONS  
1:25=25'

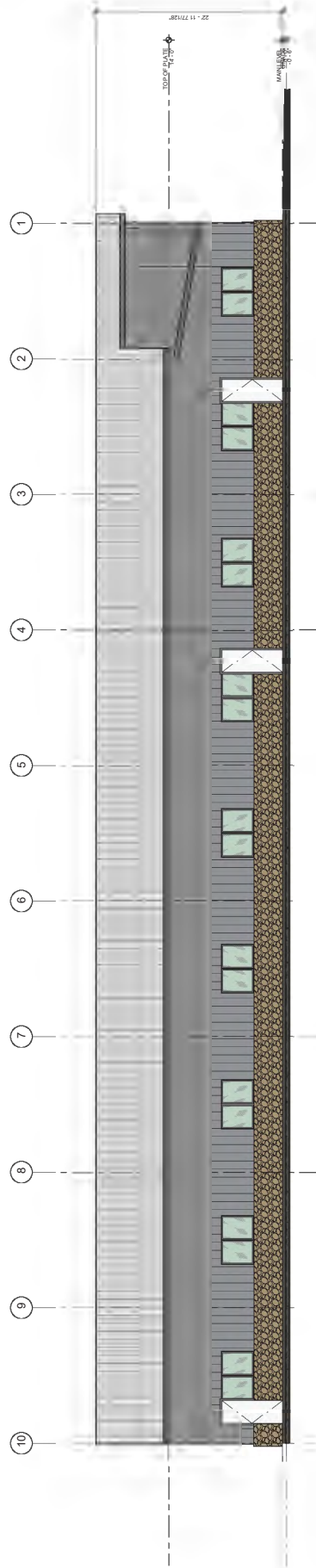
A320

KEY NOTES

- 1. CONCRETE FOOTING, REFER TO FOUNDATION PLAN FOR SIZE & REINFORCING.
- 2. CONCRETE FOUNDATION WALL, SEE STRUCTURAL FOR SIZE & REINFORCING.
- 3. ARCHITECTURAL NOTES FOR FINISH SPECIFICATION.
- 4. EMERGENCY EVACUATION ROUTE.
- 5. DRINKING FOUNTAIN, ADA AND NORMAL HEIGHT.
- 6. BREAK ROOM REFRIGERATION - PER OWNER.
- 7. BREAK ROOM SINK - PER OWNER.
- 8. BREAK ROOM DISH WASHER - PER OWNER.
- 9. BREAK ROOM CUPBOARD - PER OWNER.
- 10. LAUNDRY CLOSET - ADA COMPLIANT, 3" MAX.
- 11. LAUNDRY SINK / WIP SINK.
- 12. WATER CLOSET - ADA COMPLIANT WHERE GRAB BARS SHOWN.
- 13. URINAL.
- 14. SHOWER, ANCHORED ALUMINUM WINDOW FRAMES, C/W DOUBLE GLAZED, ARGENTILLED, LOW E COATING, CLEAR PANES.
- 15. COUNTER TOP - ELBORADO STONE.
- 16. WALLS - ELBORADO STONE.
- 17. FLOOR - ELBORADO STONE.
- 18. 24 GA. W/EN. METAL STANDING SEAM ROOF - COLOUR: WARM WHITE.
- 19. 1" X 2" - 20ga W/EN. VERTICAL STEEL SILLING FROM PRE-ENG SUPPLIER - COLOUR: SLATE GRAY, WARM WHITE.
- 20. PRE-FINISHED METAL TRIMS, FASCIA, GUTTERS AND RAIN WATER LEADER - SLATE GRAY.
- 21. GABLE TRIM AND GUTTERS - SLATE GRAY.
- 22. SCHEDULE IN BACKLIGHTING, BY OWNER, 60" TO PROVIDE STRUCTURAL SUPPORT AND ELECTRICAL SERVICES, COORDINATE WITH SUPPLIER.
- 23. 24 GA. BENT METAL CAP FLASHING (COLOUR TO PRE-ENG TRIMS).
- 24. INSULATED-HALOLOW METAL DOOR AND FRAME - COLOUR BY OWNER, REFER TO DOOR SCHEDULE.
- 25. INSULATED SECTIONAL OVERHEAD DOOR - COLOUR BY OWNER, REFER TO DOOR SCHEDULE.
- 26. DOOR LEVER, BY OWNER - COLOUR BY OWNER.
- 27. DOOR SHELTER, BY OWNER - COLOUR BY OWNER.
- 28. DOOR BUMPERS, BY OWNER - COLOUR BY OWNER.
- 29. FAUX KNOCK ON EXHAUST FAN, NON WORKING FOR ARCHITECTURAL CHARACTER.
- 30. 6" CONCRETE FILLER STEEL PIPE BOLLARD - PAINT COLOUR - YELLOW.
- 31. REMOVABLE GUARD RAILS, 1-1/2" IN ROUND, 42" HIGH, PAINT COLOUR - YELLOW.
- 32. C CHANNEL STEEL STRINGER (STAIRS).
- 33. PRE-ENG GALVANIZED PERFORATED METAL START/END LANDING.
- 34. ADA COMPLIANT GRAB BARS.
- 35. SERVICE SINK.
- 36. MOP SINK.



1 SOUTH  
SCALE: 1/4\"/>



2 WEST  
SCALE: 1/4\"/>

## **Attachment 4 – ITE Trip Generation Data for Land Use 560: Church**

(ITE Trip Generation Manual, 11<sup>th</sup> Edition)



## **Land Use: 560**

### **Church**

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#### **Description**

A church is a building in which public worship services are held. A church houses an assembly hall or sanctuary. It may also house meeting rooms, classrooms, and, occasionally, dining, catering, or event facilities. Synagogue (Land Use 561) and mosque (Land Use 562) are related uses.

#### **Additional Data**

Worship services are typically held on Sundays. Some of the surveyed churches offered day care or extended care programs during the week.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Colorado, Florida, New Hampshire, New Jersey, New York, Oregon, Pennsylvania, Texas, and Virginia.

#### **Source Numbers**

169, 170, 423, 428, 436, 554, 571, 583, 629, 631, 704, 903, 904, 957, 971, 981, 1080

## Church (560)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Sunday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 16

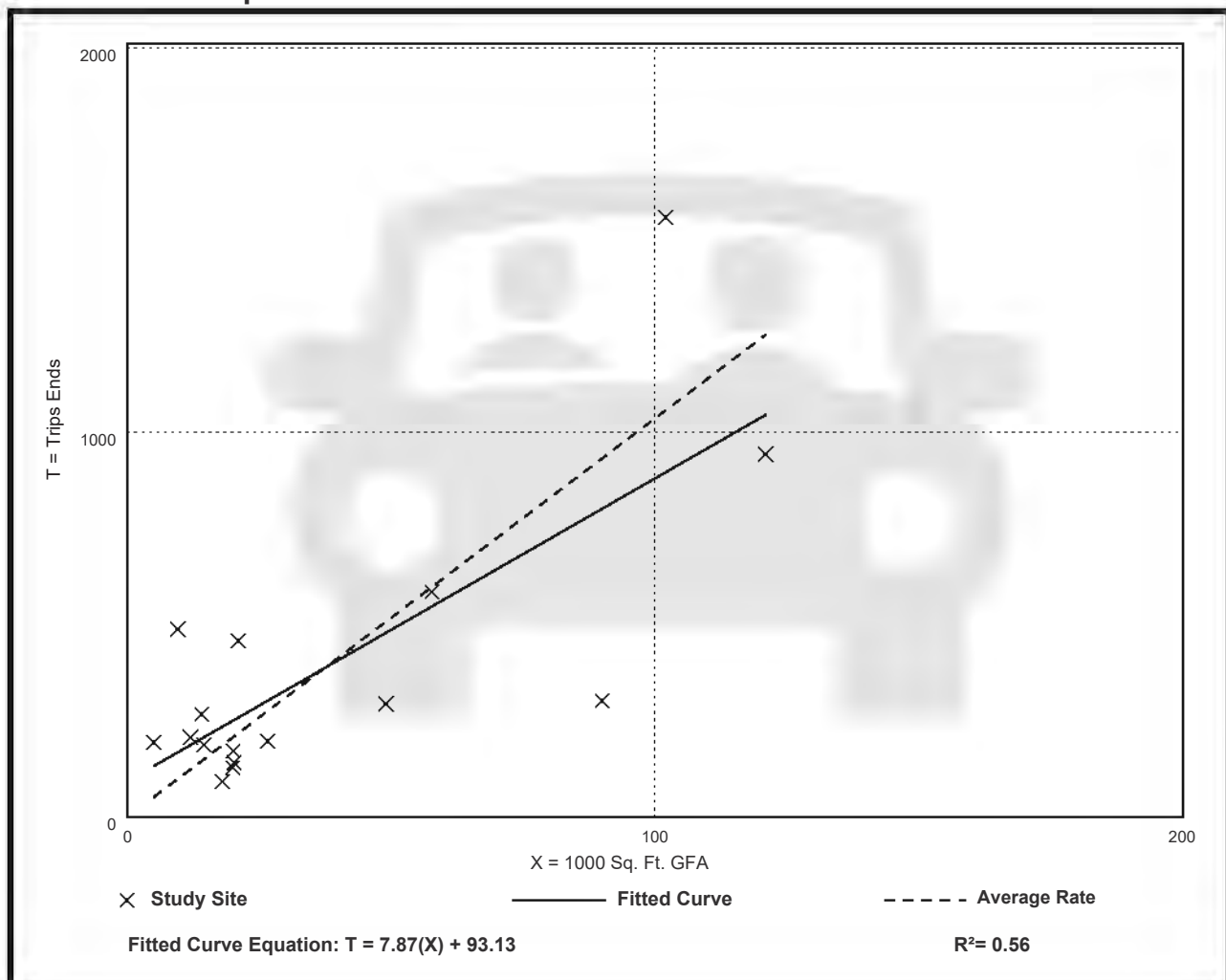
Avg. 1000 Sq. Ft. GFA: 38

Directional Distribution: 48% entering, 52% exiting

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
10.36	3.36 - 51.31	7.83

### Data Plot and Equation



## Church (560)

### Vehicle Trip Ends vs: Attendees

On a: Sunday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Attendees: 364

Directional Distribution: 50% entering, 50% exiting

### Vehicle Trip Generation per Attendee

Average Rate	Range of Rates	Standard Deviation
0.53	0.49 - 0.57	***

### Data Plot and Equation

Caution – Small Sample Size

